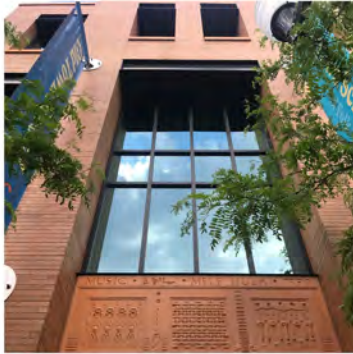


2019 MASTER PLAN



Thank You

Lincoln Square SSA #21 Staff

Rudy Flores, Exec Director

Ryan Warsing,
Neighborhood Services Director

Lincoln Square SSA Commissioners

Todd Feinberg, SSA Vice Chair, Bistro Campagne, Resident

Jason Kraus, Rockwell Partners

Ben Munro, Davis Theater/Carbon Arc Bar

Melaina Prest, SSA Chair, Candea Development

Anthony Qaiyum, Merz Apothecary/The Shops at Merz

John Ratzenberger, Gateway to Learning

Nick Yassan, RANY Management

LSRCC Board of Directors

Ed Kaczmarek
LSRCC President,
Urban Pooch Canine Life Center

Scott Friedland
Timeless Toys

Sean Cooper
Cooper Financial Planning

Ad-hoc Advisory Committee

Allen Schild, Heart of Lincoln Square Neighbors

Adam Kingsley, Heart of Lincoln Square Neighbors

Stephen Reynolds, Winnemac Park Neighbors

Connie Barnes, Greater Rockwell Organization

Lucinda Dieker, Greater Rockwell Organization

Tom Kosinski, Greater Rockwell Organization

Abbey Butkin, Greater Rockwell Organization

Adam Kashuba, Gross Park Advisory Council

John Ratzenberger, Gateway to Learning, SSA

Dave Zibell, Old Town School of Folk Music

Jason Kraus, Rockwell Partners, SSA

Melaina Prest, Akasha Development, SSA

Todd Feinberg, Bistro Campagne, SSA

Scott Friedland, Timeless Toys

Bob Farster, Resident

Prepared by **Teska Associates Inc. | Sam Schwartz Engineering | Goodman Williams Group**

TABLE OF CONTENTS

Introduction + Executive Summary	05
CHAPTER 1 LISTEN: Support Community Engagement	21
CHAPTER 2 TRANSPORT: Improve Connections for all Modes	27
CHAPTER 3 PROGRESS: Support Local Businesses + Guide New Development	47
CHAPTER 4 REINFORCE PLACE : Foster Placemaking + A Vibrant Community	79
CHAPTER 5 IMPLEMENT: Develop Projects Strategically	103
APPENDIX	113
Meeting & Event Summaries	
Neighborhood Poll Summaries	
Existing Urban Design Conditions	





INTRODUCTION + EXECUTIVE SUMMARY

INTRODUCTION

To call Lincoln Square a special place would be an understatement – visitors know it as a hip neighborhood with a quaint, walkable shopping street, unique local restaurants and lively community events. For the 40,000+ residents, businesses and institutions who call Lincoln Square home, this is also a neighborhood of folks who care deeply about their community and the happenings within. An extension of the community, the Lincoln Square Ravenswood Chamber of Commerce (LSRCC) and Special Service Area #21 (SSA) are strong supporters of Lincoln Square’s commercial districts and the neighborhoods which abut them.

Lincoln Square’s SSA #21, which is overseen by Chicago’s Department of Planning and Development, is a property tax district that provides funding and support for local initiatives, such as beautification in the public way, business retention and attraction initiatives, transit improvements, public art and community branding.

After reconstituting in 2016, SSA #21 was renamed the Lincoln Square Neighborhood Improvement Program. Shortly following the reconstitution, the SSA developed a Strategic Plan. The Strategic Plan addressed community demographics, a revised organization branding scheme, revised organization bylaws and SSA responsibilities, a comparative study of other successful business districts and redevelopment site visioning. The Strategic Plan was completed in 2017 and laid the groundwork for the Lincoln Square Master Plan described herein.

In the summer of 2018, SSA #21 selected a team of consultants led by Teska Associates, Inc. to lead a Master Plan for the SSA and greater community. Teska’s planning partners include Sam Schwartz Consultants, who provided expertise in transportation planning, and Goodman Williams Group, who consulted on items related to business retention and attraction. Central to the Master Planning effort was a robust community outreach process that solicited inputs from residents, businesses and organizations in Lincoln Square. The Master Plan identifies objectives, projects and actions distilled from thousands of comments received.

STUDY AREA

Lincoln Square is generally located between the Chicago River to the west, Foster Avenue to the north, Montrose Avenue to the south and Leavitt Street to the east. The commercial corridors served by SSA #21 are the focus areas of this study and include the following:

- **West Lawrence Avenue from the Chicago River (west) to North Leavitt Street (east)**
- **West Montrose Avenue (north side only) from the Chicago River (west) to North Leavitt Street (east)**
- **North Lincoln Avenue from West Montrose Avenue (south) to West Foster Avenue (north)**
- **North Western Avenue from West Montrose Avenue (south) to West Ainslie Street (north)**
- **North Rockwell Avenue from West Eastwood Avenue (south) to West Leland Avenue (north)**

Lincoln Square is located within the 40th and 47th Wards.

Ward Map





- SSA #21 Boundary
-  Brown Line Station
-  Point of Interest / Event Space



Image Credit: C. William Brubaker Collection UIC Library

HISTORY

Similar to Chicago neighborhoods dating back to the 1850's, Lincoln Square was settled as a farming community. Early commercial agriculture centered on flowers, pickles and celery. Farmers immigrating from Germany and Poland used Little Ford Road (now Lincoln Avenue) to transport goods to the Chicago marketplace. As the local farming industry grew, so did the workforce and the concentration of local businesses.

Residential developments followed suit as the community became an attractive neighborhood for worker housing. As public transportation emerged in Lincoln Square, first with electric streetcars in the 1890's, followed by the Elevated train system in 1907, new bungalows, two-flats and apartment buildings began to pop up all over the area. The next influx of development occurred after World War II when Lincoln Square became recognized as the "new Greektown". The "old Greektown" was displaced by construction of the Eisenhower Expressway and University of Illinois at Chicago.

As many storefronts fell vacant following the war, local initiatives sought to revitalize the area. Among the most controversial – and perhaps the most successful – was the development of the Lincoln Square mall in the late 1970's. During this time, Lincoln Avenue between Lawrence Avenue (north) and Leland Avenue (south) was converted from a two-way to a one-way street. This area is known locally as The Square. The LSRCC, founded in 1949 as the "Lincoln Square Chamber of Commerce," worked to evoke an Old-World European style street. The success of this effort is evident today, and Lincoln Avenue - with its pedestrian friendly amenities and beloved small businesses - has become a model for other streetscapes.

However, Lincoln Square has seen its fair share of development challenges. Recent trends in the residential market include the deconversion of two and three-flats to single family homes, and suburban style commercial developments continue to limit pedestrian use and comfort. Public plazas and streets need functional and aesthetic improvements to meet the demands of this growing community. Various modes of transportation need to be better supported, including CTA bus and rail, bicycle and pedestrian access throughout.

EXECUTIVE SUMMARY

Listen | Support Community Engagement

The Master Planning team made sure to engage the community on many levels. An Advisory Committee of community stakeholders provided much needed insights and plan reviews. Additional stakeholder interviews were conducted with local businesses, agencies, organizations, and the development community. The community at large was engaged via an interactive website, online polls, activities at Apple Fest and a community open house meeting. Suffice it to say, there was no shortage of creative ideas, constructive comments and honest reactions throughout the planning process. The Master Plan presented herein is the culmination of these outreach efforts. Though this plan is ostensibly “final”, the community is encouraged to continue sharing issues and opportunities with the SSA and Aldermanic offices.



Transport | Improve Connections for All Modes

Lincoln Square is a well connected community with access to the Western and Rockwell Brown Line CTA Stations, bus routes, Divvy stations, bike routes (both existing and planned), and connected sidewalks. Yet the safety and connectivity of pedestrian and bicycle transit, in particular, must be improved to meet the growing needs of this vibrant community and connect residents to destinations.

Key transportation objectives explored throughout the plan include:

- **Improve safety for people walking and biking**, building upon the City of Chicago’s Vision Zero program and enhancing roadway corridors and transit station areas
- **Improve traffic flow**, optimizing roadway intersection signal timing, encouraging a diversity of transit models and creating bike facilities that improve Lincoln Square’s reach to surrounding areas
- **Reinforce a sense of place**, maintaining special events spaces, enhancing streetscapes at key locations and installing gateway and wayfinding signage that improve connectivity and promote Lincoln Square’s unique character
- **Improve connections**, developing low stress bike routes, and prioritizing regional bike routes that connect Lincoln Square to the greater Chicagoland area

Progress | Support Local Businesses and Guide New Development

Lincoln Square is recognized throughout the Chicagoland area as a unique neighborhood, filled with locally owned shops, vibrant community events, and walkable shopping and dining areas. While The Square is widely recognized as a destination for shopping, dining and gathering, the surrounding corridors also offer a range of community destinations. As these corridors continue to evolve, the characteristics of new development should further enhance walkability, local character, diversity and connection to the cultural richness of Lincoln Square.

Key construction and development objectives explored throughout the plan include:

- **Preserve community character**, supporting event and festival programming and improving access to local businesses
- **Support local businesses development**, encouraging developers to secure local tenants, coordinating City/County incentives for small businesses, expanding local events and developing pop-up shops
- **Encourage a range of residential developments**, including transit oriented and dense developments whose new residents would support local businesses
- **Promote quality development** that reflects community vision and corridor character, partnering with Aldermanic offices and the development community to negotiate designs that are architecturally appropriate and in keeping with the context of Lincoln Square



Reinforce Place | Foster Placemaking + A Vibrant Community

Lincoln Square's unique vibe is partly due to the SSA's commitment to fostering a strong sense of place and improving the public realm with new infrastructure, art, and catalytic business attraction programs.

The SSA provides clean and attractive spaces for the Chamber of Commerce to program special activities. The 14+ community events put on by the LSRCC include the Lincoln Square Apple Fest, Farmers Markets, the Square Roots Festival and many more. During many of these fests, public streets and plaza spaces are quickly transformed to entertain large crowds, and must (just as quickly) be transformed back to accommodate deliveries, trash removal, and local business traffic.

Other site specific installations have a large impact on the environment with low infrastructural cost. The SSA organizes, manages and often funds many of these projects, including the installation of street planters and hanging flower baskets, vinyl banners, bicycle racks, murals, and other art projects like the "Parade of Abes" conversion of outmoded parking meters.

The SSA will partner with other local agencies on larger scale infrastructure projects, such as the Lawrence Avenue Streetscape, which will be maintained by the SSA after being constructed by CDOT.

Key placemaking objectives explored throughout the plan include:

- **Reclaim sidewalks**, exploring the creation of a bench/ planter art program, developing mini-plazas or (SQU)Areas, and enhancing existing plaza spaces, alleyways and walk thrus beneath the Western Avenue CTA station.
- **Reinforce placemaking**, installing colorful street banners and holiday decor, improving the safety and functionality of existing plaza areas, and upgrading the Lincoln Avenue streetscape to better support community fests and events.
- **Improve corridor appearances**, organizing a blade sign program to heighten local business visibility, adding parklets and widened sidewalk areas at key locations, and installing wayfinding signage and business directory kiosks to promote local businesses and better connect corridors.
- **Identify and develop near term projects** such as murals, bike racks, and additional interventions designed to achieve maximum impacts without costly infrastructural changes.
- **Identify and develop long range projects**, including public way, roadway, plaza and transit hub enhancements to promote placemaking and community interaction. Long range projects identified herein involve partnerships with local agencies, such as the CTA and CDOT.

DEMOGRAPHICS

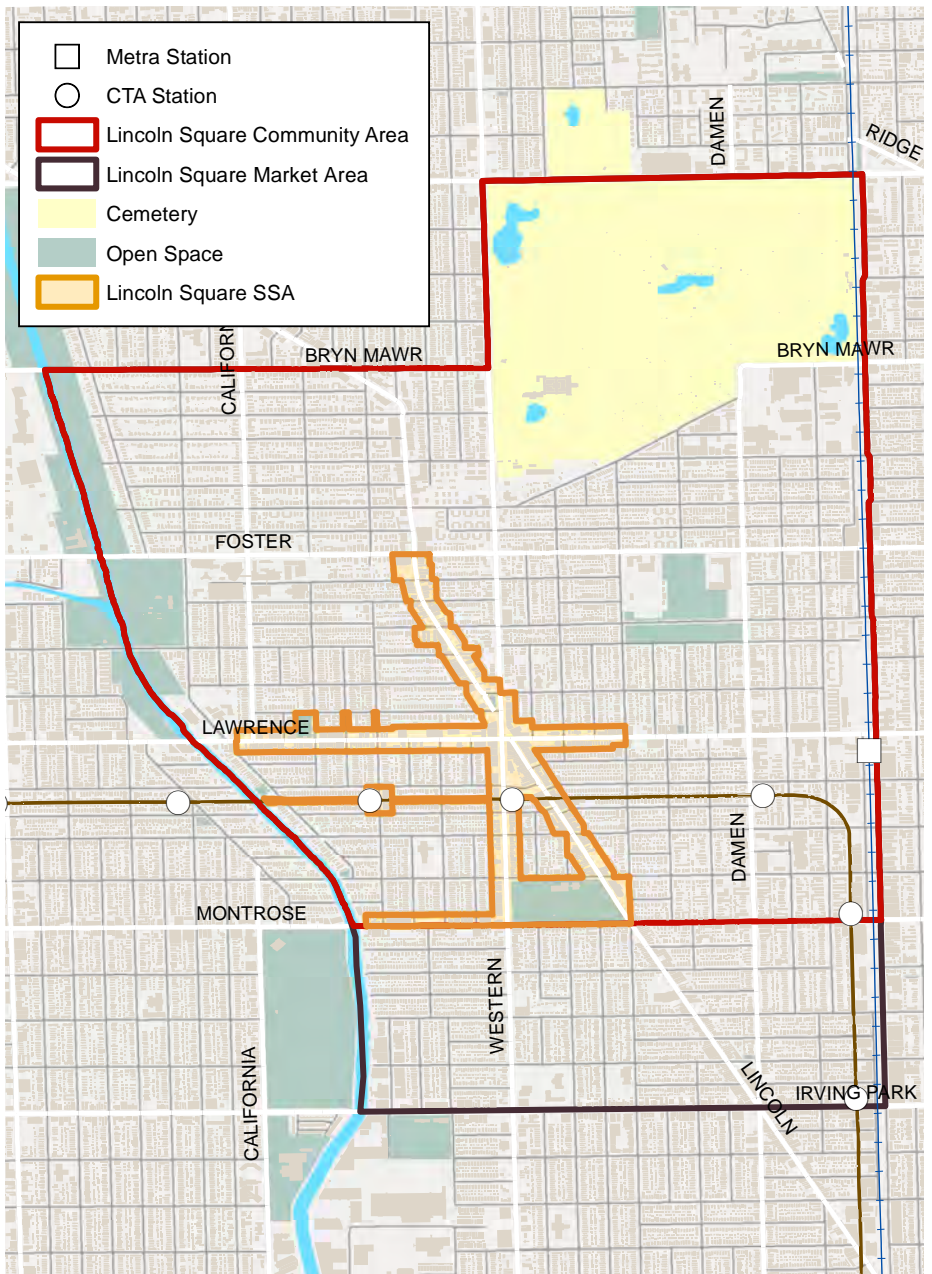
Analysis Boundaries

The Lincoln Square Market Area is defined as the area between the Chicago River to the west, Peterson Avenue to the north, Ravenswood Avenue to the east and Irving Park Road to the south. Lincoln Square's shops and restaurants draw patrons from the wider Market Area.

The Lincoln Square community area is the area generally located between the Chicago River to the west, Peterson Avenue to the north, Ravenswood Avenue to the east and Montrose Avenue to the south. The community area is a City defined area that aligns with census tracts and is used for planning purposes.

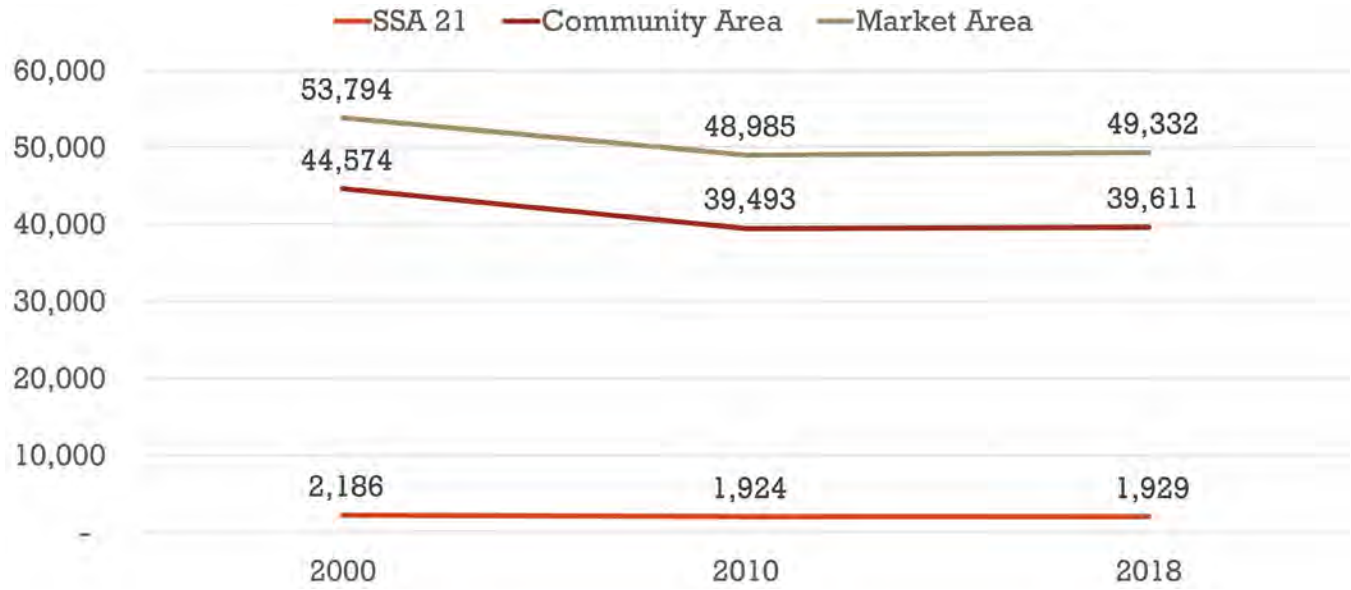
The Lincoln Square SSA is the area located along the SSA's commercial corridors. It contains the hubs of activity and commercial businesses which draw the community together.

The demographic information referred to in this report are from Esri Business Analyst and include 2000 and 2010 Census data as well as Esri's 2018 estimates.



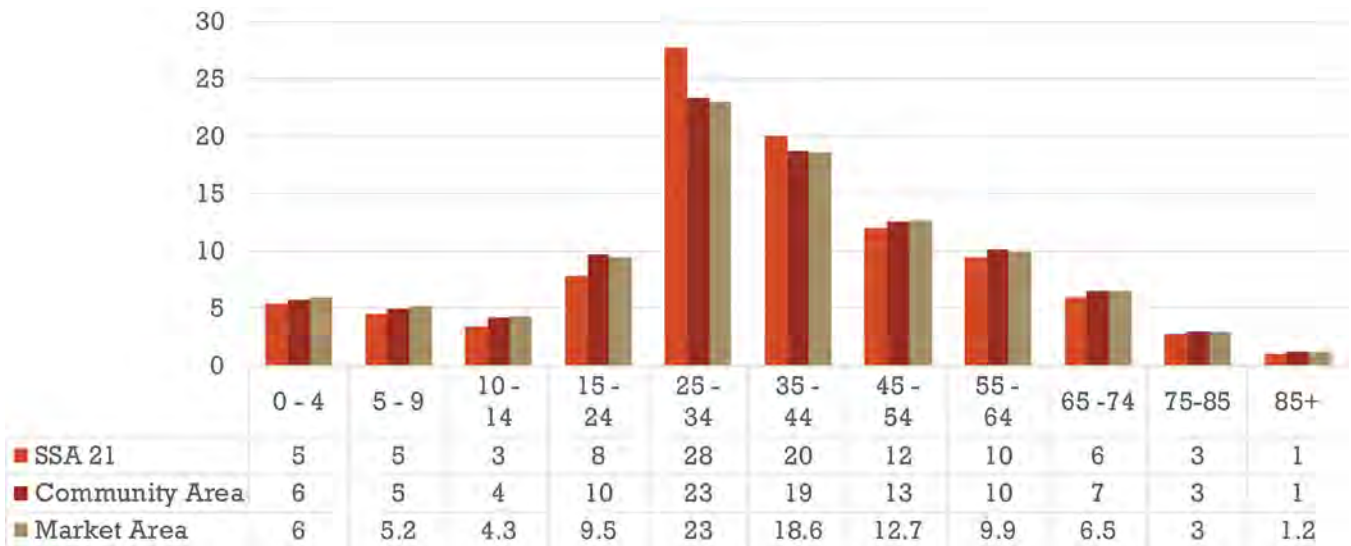
Population

In the ten years between 2000 and 2010, the population of Lincoln Square declined. However, losses have since stabilized. Some of this change is attributable to the deconversion of multi family homes into single family homes.



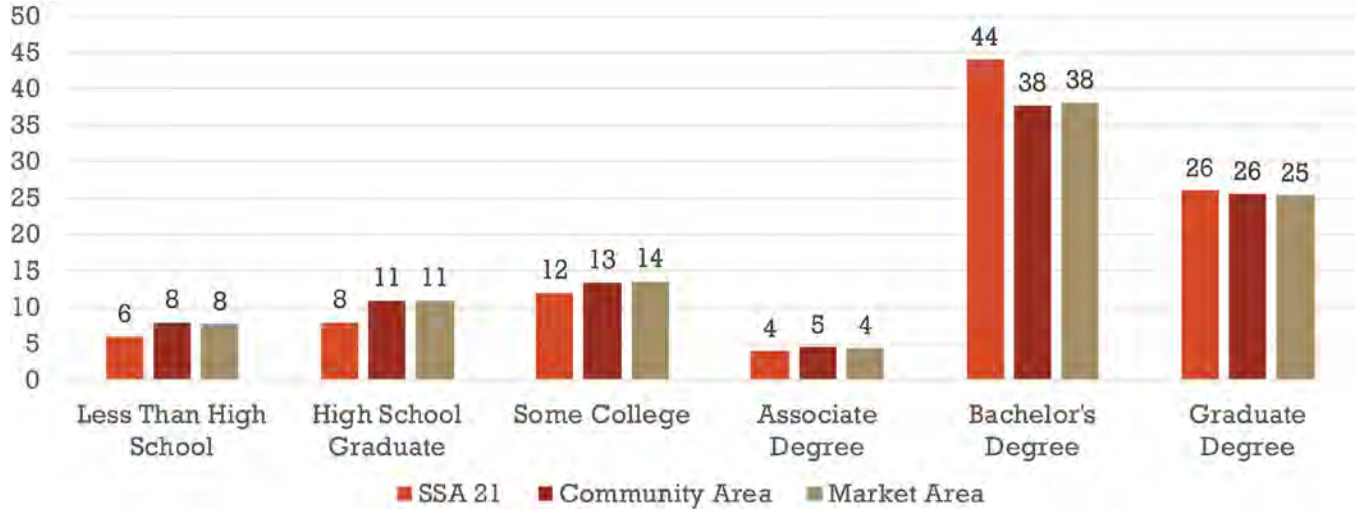
Median Age

The Market Area of Lincoln Square is slightly older than the City of Chicago, with a median age of 34.4. Roughly 48% of the market population is under age 35, which is 3% lower than the City of Chicago as a whole.



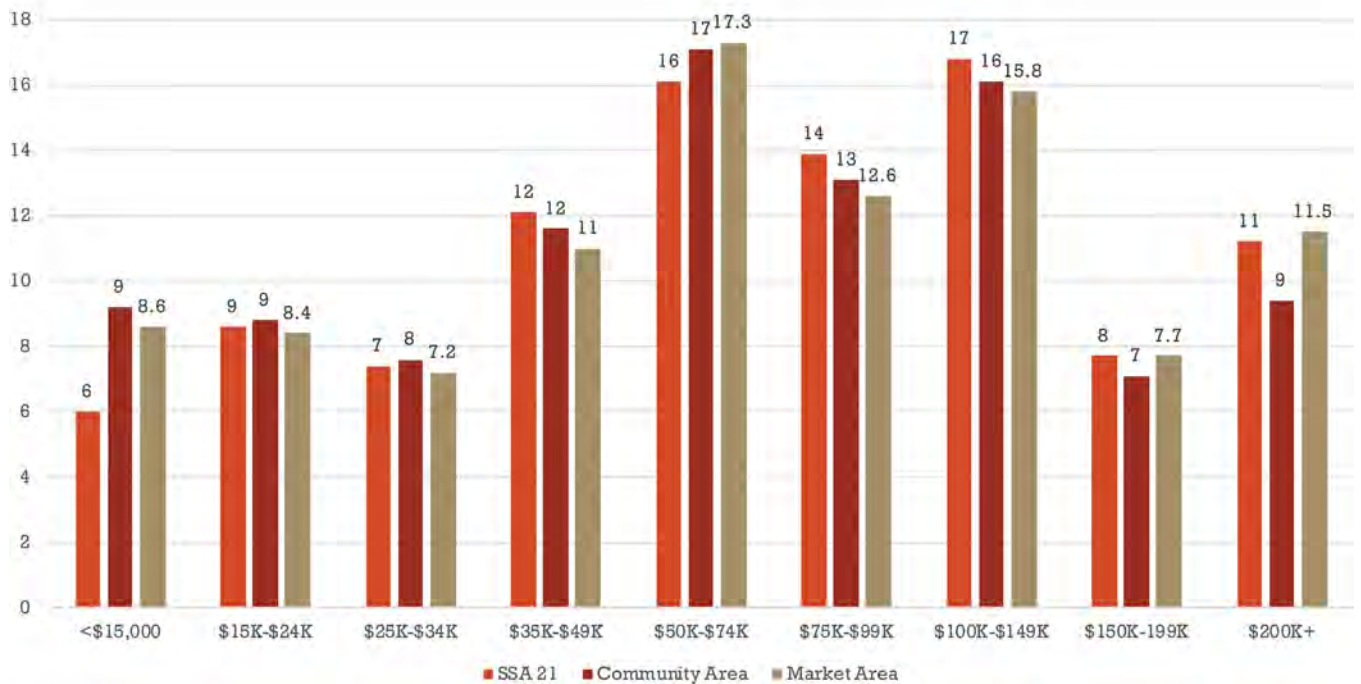
Education

The Market Area of Lincoln Square is highly educated. The percentage of population age 25 and older with at least a bachelor's degree (63%) is 21% higher than the City as a whole (42%).



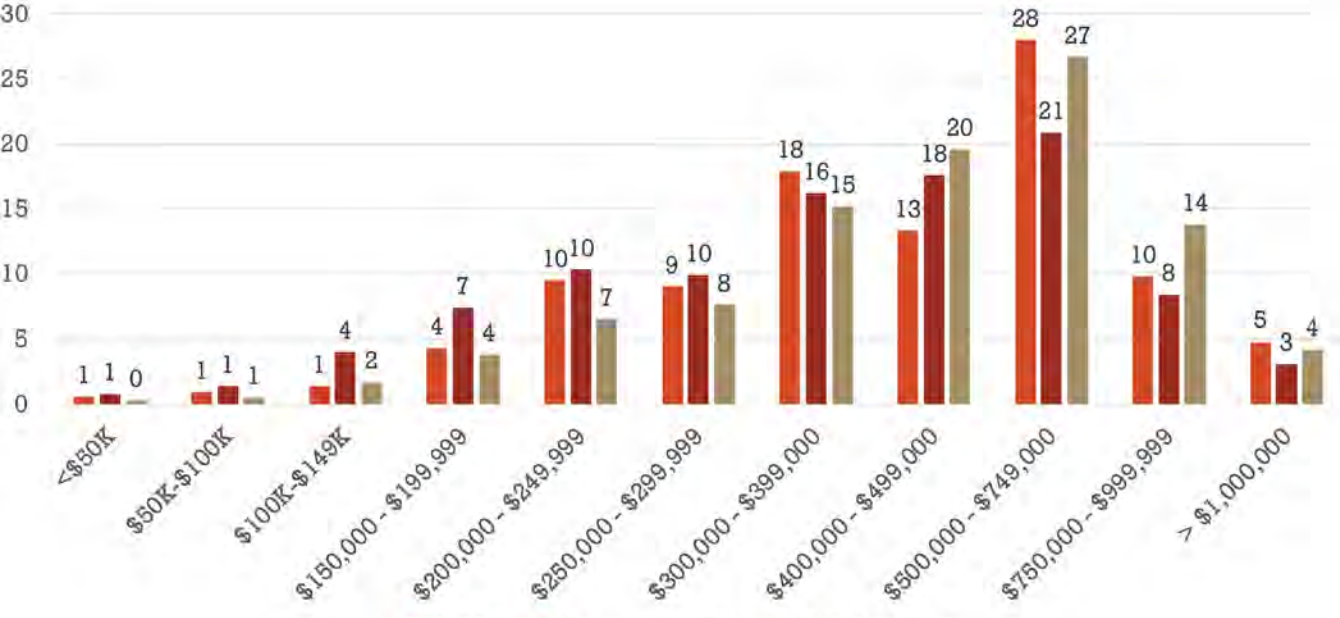
Median Household Income

The median income for Chicago is about \$53,000. This is \$17,000 less than the Market Area.



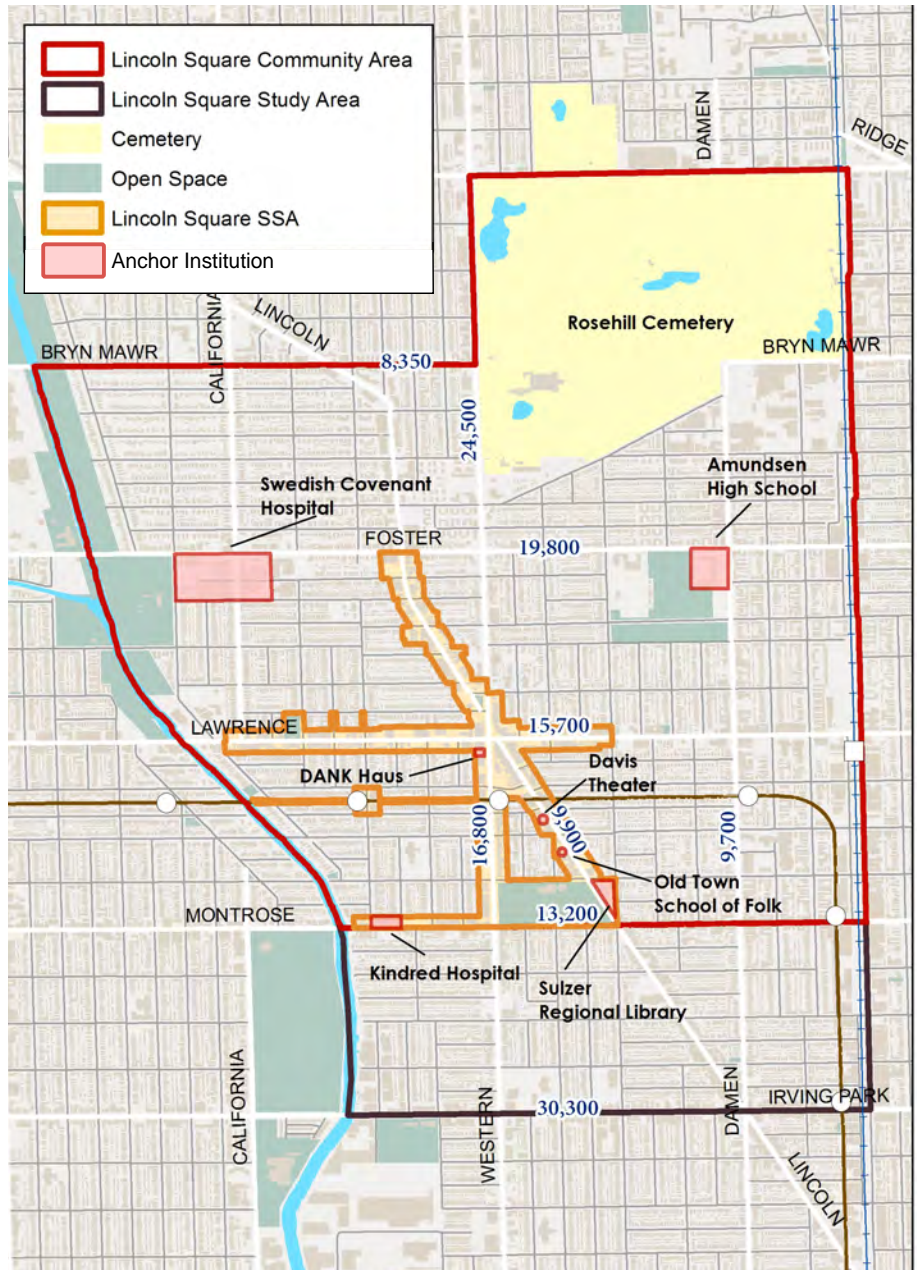
Median Home Value

According to 2018 data from the Multiple Listing Service, 45% of homes in the market area are valued above \$500,000. This includes attached and detached housing. The median home price in the market area is \$422,621, which is 63% higher than the City of Chicago’s median home price of \$258,868. Within the Lincoln Square market area, only 13% of homes are valued below \$250,000.



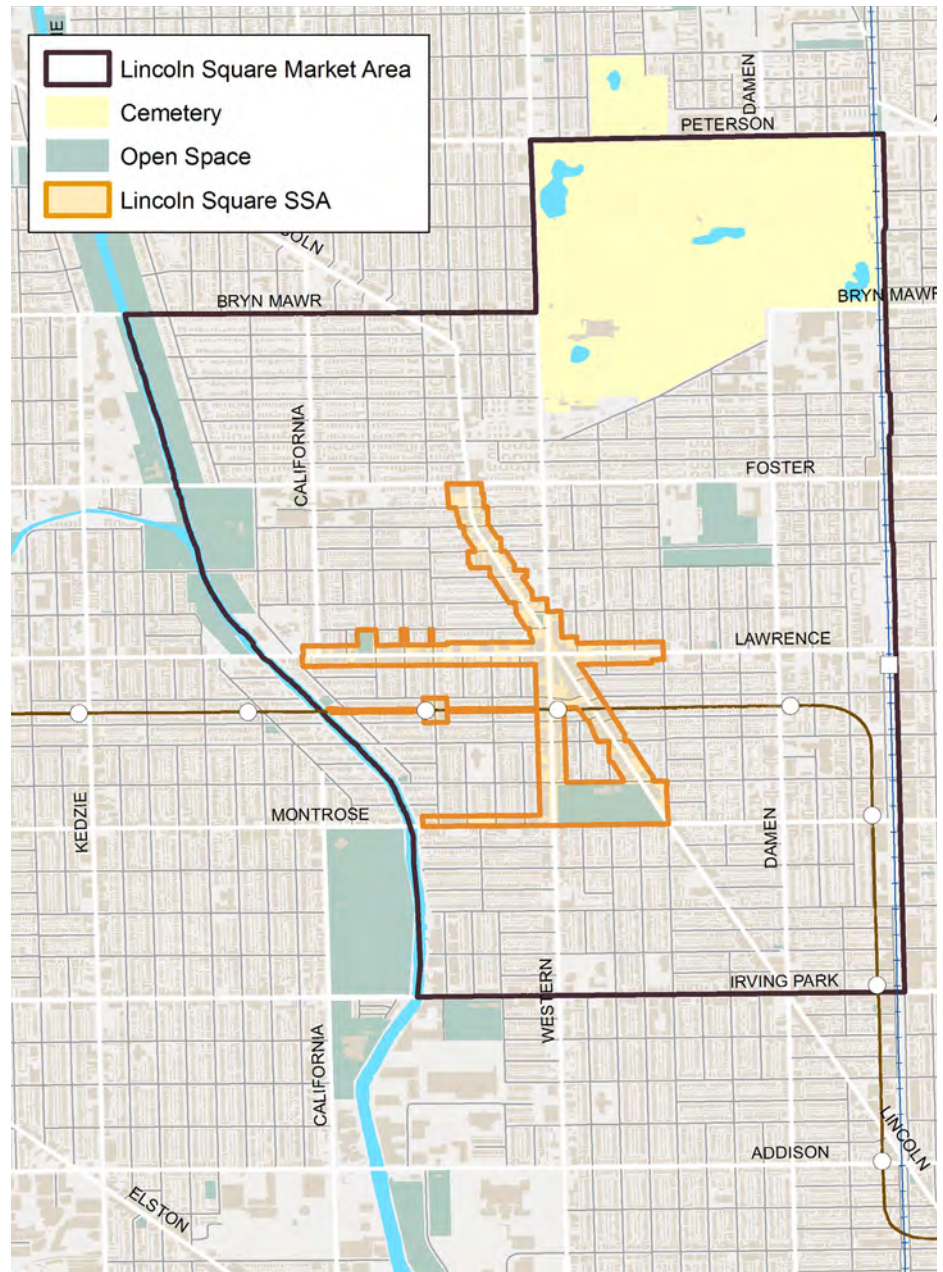
Anchors and Traffic Counts

Lincoln Square revolves around several large anchors. Swedish Covenant Hospital, for example, employs 2,500 people and has nearly 11,000 discharges per year. Amundsen High School is the daytime home of over 1,100 students and more than 125 employees. The Old Town School of Folk Music draws approximately 100,000 patrons per year. Kindred Hospital has 164 beds for extended recovery and employs more than 500 people. The absence of any one of these anchors would be detrimental to the Lincoln Square community. The SSA will therefore strive to work with and benefit these institutions.



Commercial Real Estate Indicators

The Market Area includes 3.2 million square feet of commercial space according to CoStar, a leading supplier of real estate information. This includes 70,000 square feet alone in the recently constructed Mariano's grocery at the corner of Lawrence and Ravenswood Avenues. More than 150,000 square feet of positive absorption has occurred since 2013. Although rents vary depending on location, average net rents are below \$25.00 per square foot.

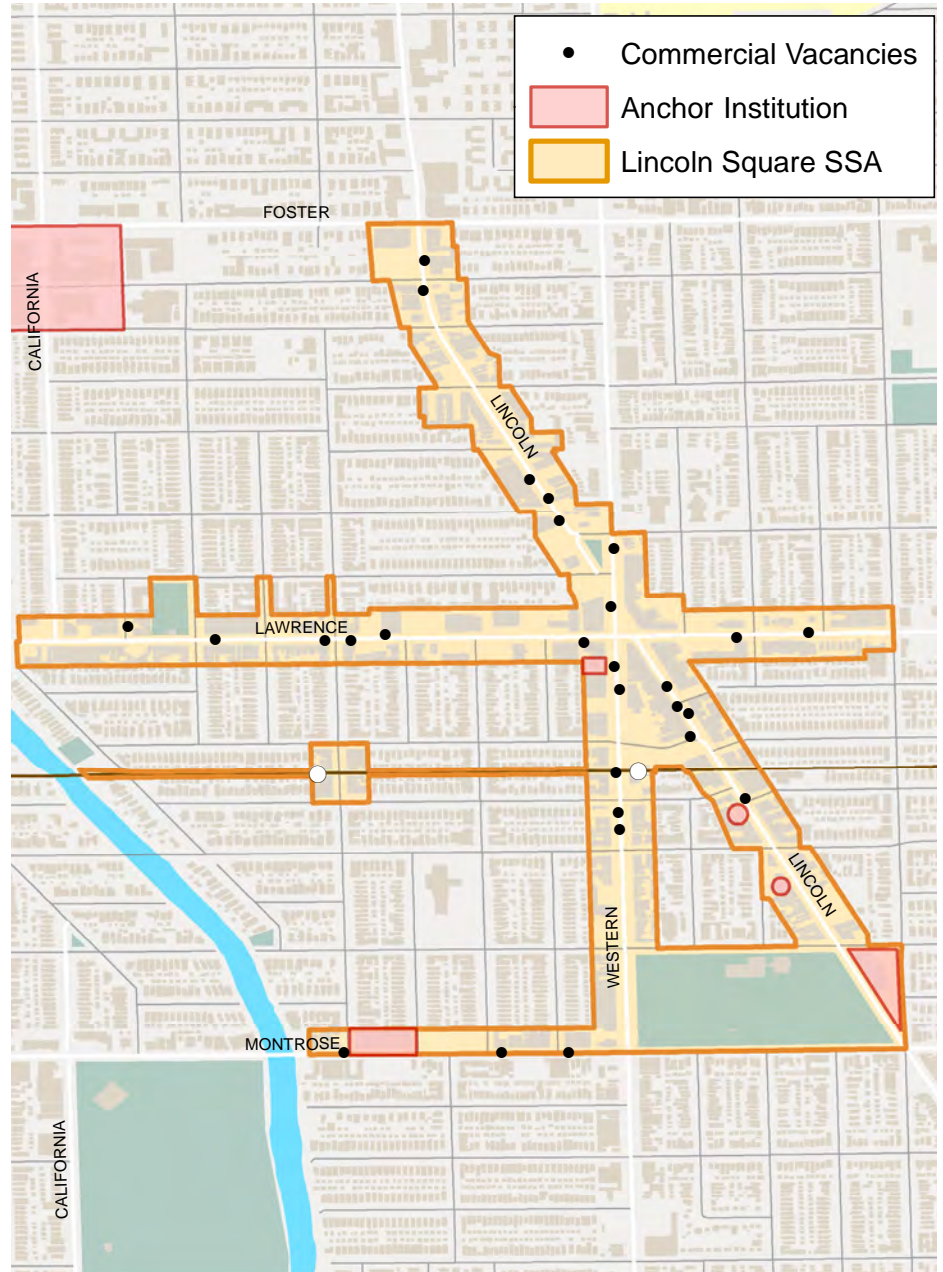


Lincoln Square Commercial Indicators				
Geography	Commercial SF	5 Yr Net Absorption	NNN Rent	Direct Vacancy Rate
Market Area	3,216,837	156,351	\$23.93	2.6%
SSA	1,239,079	32,427	\$20.37	4.3%

Source: CoStar

Vacancies in the Study Area

Vacancies are located throughout the SSA and are not clustered in specific geographic locations. There is some concentration of vacancies close to the intersection of Lincoln Avenue, Lawrence Avenue and Western Avenue, likely because of the higher number of storefronts in this area. Fewer vacancies exist near the Davis Theatre and the Old Town School of Folk Music south on Lincoln Avenue. No vacancies are listed in available data sources for the Rockwell Station area, however anecdotal evidence points to vacant storefronts there that are not actively marketed.



Gap Analysis

Although Lincoln Square contains a wide diversity of business offerings, a gap analysis for the market area suggests additional spending potential in most retail trade categories as well as Food and Drink. Gap Analysis estimates the difference between retail demand from households in the Market Area and retail sales reported from businesses in that area. Internet sales and revenue from companies with four or fewer employees are not included in this analysis.

Gap Analysis for Lincoln Square Market Area

Summary Demographics

2018 Population	49,332
2018 Households	22,936
2018 Median Disposable Income	\$54,322

Industry Summary	Demand (Retail Potential)	Supply (Retail Sales)	Retail Gap
Total Retail Trade and Food & Drink	\$3,790,809,778	\$2,183,656,429	\$1,607,153,349
Total Retail Trade	\$3,389,704,793	\$1,849,164,989	\$1,540,539,804
Total Food & Drink	\$401,098,985	\$334,491,440	\$66,607,545

Source: Esri Business Analyst. Oct. 2018

Recently Completed and Planned Developments

Lincoln Square has seen a surge of development in recent years. Since the beginning of 2015, 465 new units have been constructed as part of 29 new projects. Of these developments, 17 include commercial spaces on the ground floor. The highest concentration of development exists along Lawrence Avenue and Lincoln Avenue, north of Lawrence. The largest projects, both planned and constructed, include:

- 150 unit Ravenswood Station project (Mariano’s Grocery, other retail)
- 166 unit project permitted for the southwest corner of Ravenswood and Lawrence
- 59 unit redevelopment of former Sears property on Lawrence Avenue; ground floor commercial space to be occupied by DeVry University
- 440 unit project at the southwest corner of Eastwood and Western (The Westerner)
- Planned conversion of former Ravenswood Hospital into affordable senior housing

Key Takeaways

- Lincoln Square's commercial corridors are all unique. Future development should be tailored to meet the needs of each, while also building upon the energies of the greater whole.
- Transit oriented development is to be encouraged. With existing amenities, new development would strengthen commercial demand without significant traffic impacts.
- Housing should be made more diverse in terms of composition and affordability, including both affordable and market rate housing and greater housing for senior citizens.
- Protect the character and programming of The Square. It is the hub of activity and serves as an important place for the community to gather.





CHAPTER 1 | LISTEN

SUPPORT COMMUNITY ENGAGEMENT

CHAPTER 1 ENGAGEMENT

Advisory Committee

SSA Commission

Stakeholder Interviews:

Lawrence Hall

Cambodian Association

Sulzer Library

Levy Center

DANK Haus

Old Town School

Welles Park (Chicago Park District)

Forward Chicago

Local businesses and restaurants

40th & 47th Ward Aldermanic Offices

Local schools

Local developers

Chicago Dept of Transportation (CDOT)

Chicago Transit Authority (CTA)

Public Workshops:

10/07/2018 Apple Fest Idea Booth

05/30/2019 Public Open House

Online Engagement

360 Website Subscribers

9,700+ Website Visits

584 Idea Contributions

280 Mapped Insights

1,700 Poll Responses

2,000+ Poll Comments

Additional information about meetings and events may be found in the Appendix.



COMMUNITY ENGAGEMENT

Lincoln Square residents, businesses, and stakeholders – both longstanding and new – care deeply for the neighborhood’s walkable shops and restaurants, events and festivals, access to transit, scale and style of vintage architecture, cultural institutions, and overall vibe. Throughout the course of this assignment, this engaged community shared many opinions about connectivity, local business, housing, community placemaking, and the overall look, feel and flow of Lincoln Square.

Following the successful completion of Lincoln Square’s Strategic Plan in 2018, SSA #21 set out to develop a Master Plan to help guide commercial corridors and prioritize SSA resources. Neighborhoods typically update their master plans every 5-10 years, reevaluating the areas and issues that need most attention.

COMMUNITY PARTNERS

A Project Advisory Committee was created at the start of Master Planning, including a variety of local organizations and community members with widespread networks. The Advisory Committee included the following community partners:

- Heart of Lincoln Square Neighbors
- Winnemac Park Neighbors
- Greater Rockwell Organization
- Gross Park Advisory Council
- Gateway to Learning
- Old Town School of Folk Music
- Rockwell Partners
- Candea Development
- Bistro Campagne
- Timeless Toys
- 40th and 47th Ward Aldermanic Offices

Advisory Committee members were invited to help inform goals and ensure the planning team factored in needs unique to each organization’s geography and constituents. The diverse touch points of the Advisory Committee enhanced engagement, and Committee members acted as “precinct captains” who helped their peers participate in the process.

TOOLS + TOUCHPOINTS

Throughout the 10-month community engagement process, a wide variety of online and in-person outreach tools were used to provide ample opportunities for the community to share ideas. These included:

- Focus groups
- Stakeholder interviews
- Community meetings
- Pop-up idea booth
- Social media marketing
- Print promotions
- Newsletters and online engagement via the Lincoln Square Ravenswood Chamber of Commerce
- Dedicated project website: www.YourLincolnSquare.org



Help Plan Our Neighborhood! Share Your Wishes + Ideas Today

WWW.YOURLINCOLNSQUARE.ORG

SYNTHESIZING INPUT

Collaborative tools hosted on the project website included an idea posting platform with the ability to up-vote or “heart” ideas of others, a comment map, and three neighborhood polls. Engagement highlights including comments, metrics, photos and poll results are incorporated throughout this plan.

The SSA #21 Commission was responsible for listening to all feedback and moving forward with the Master Plan presented herein. The Commission will prioritize objectives and recommendations it believes will have the most positive impact on the community.

The Master Plan will serve as a guide for the SSA to pursue both short and long range projects, working in partnership with local and citywide organizations. The Master Plan should be considered a living document. It will evolve over time based on resources, feasibility and new opportunities and challenges that might arise.

Certain projects are currently underway, or are a continuation of past efforts, while other projects are completely new ideas. Some, like the renovation of the Western Avenue Brown Line station, plaza and walkway beneath the elevated tracks, will require partnerships with Aldermanic offices, local agencies, the business community and neighborhood at large. Other projects, such as business directory kiosks and mosaic enhancements to existing planter pots, can be accomplished in the near term to help make Lincoln Square’s streets and public spaces more inviting and creative.

LINCOLN SQUARE LOVES...

Methodology: This feedback is based on the results of three neighborhood polls. Respondents were asked to prioritize those items they like most about Lincoln Square.



The walkable environment (79%)



Local shops and eateries (71%)



Access to Metra/CTA (37%)



Sense of community (34%)



Preserve historic Chicago-style architecture (open comments)



Buildings that are 2-3 stories in scale (open comments)

Community polls were conducted throughout the assignment to understand a variety of issues related to the neighborhood:

Poll #1 focused on general concepts and themes surrounding the neighborhood (563 inputs)

Poll #2 focused on site specific projects and design enhancements (695 inputs)

Poll #3 delved further into design guidelines for building development (487 inputs)

LINCOLN SQUARE WANTS...

Methodology: This feedback is based on the results of three neighborhood polls. Respondents were asked to prioritize those items they want to see in Lincoln Square.



Address traffic congestion / pedestrian / bike amenities (94%)



Public art, culture and history (38%)



More landscaping (37%)



Support for public gathering places (32%)

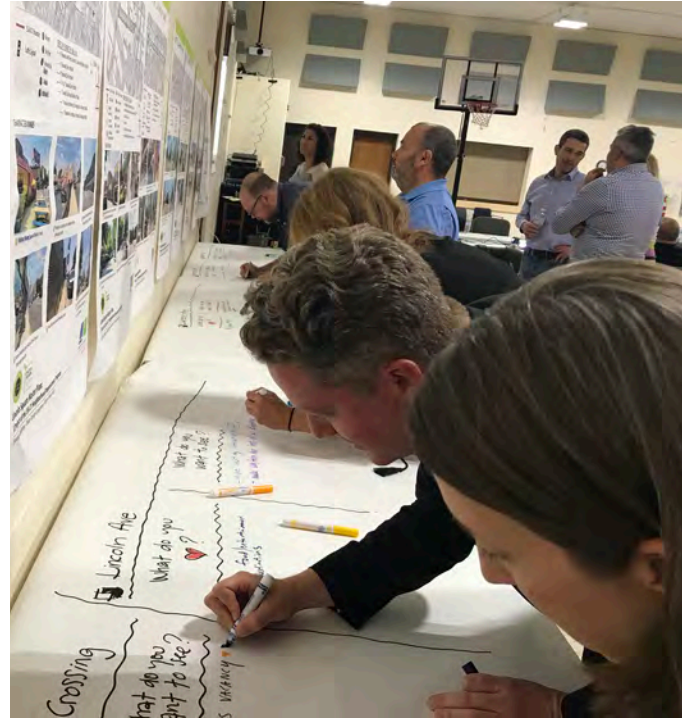


Affordable housing (42%)



Consistent street patterns and setbacks that support a more welcoming and vibrant pedestrian environment. (open comments)





The inputs received provided significant direction to guide the Master Plan development. The planning team and SSA are grateful to the community for taking the time to participate in several outreach activities and polls.

OUTREACH FUNNEL



Neighborhood Polls

- 1745 total poll responses
- 1835 comments - poll 3: urban design
- 156 comments - poll 2: placemaking
- 200 comments - poll 1: likes + needs

Website Ideas

- 534 idea contributions (hearts)
- 280 mapped comments
- 150 apple fest insights

Dialogue + Check-Points

- 25 Stakeholder Interviews
- 05 Committee / Commission Meetings

Establish Themes + Findings

- Market :: Transportation :: Urban Design
- Draft Strategies



CHAPTER 2 | TRANSPORT

IMPROVE CONNECTIONS FOR ALL MODES

CHAPTER 2 TRANSPORT

Objective 1:
Improve Safety for People Walking
and Biking

Objective 2:
Improve Traffic Flow

Objective 3:
Reinforce a Sense of Place

Objective 4:
Improve Connections

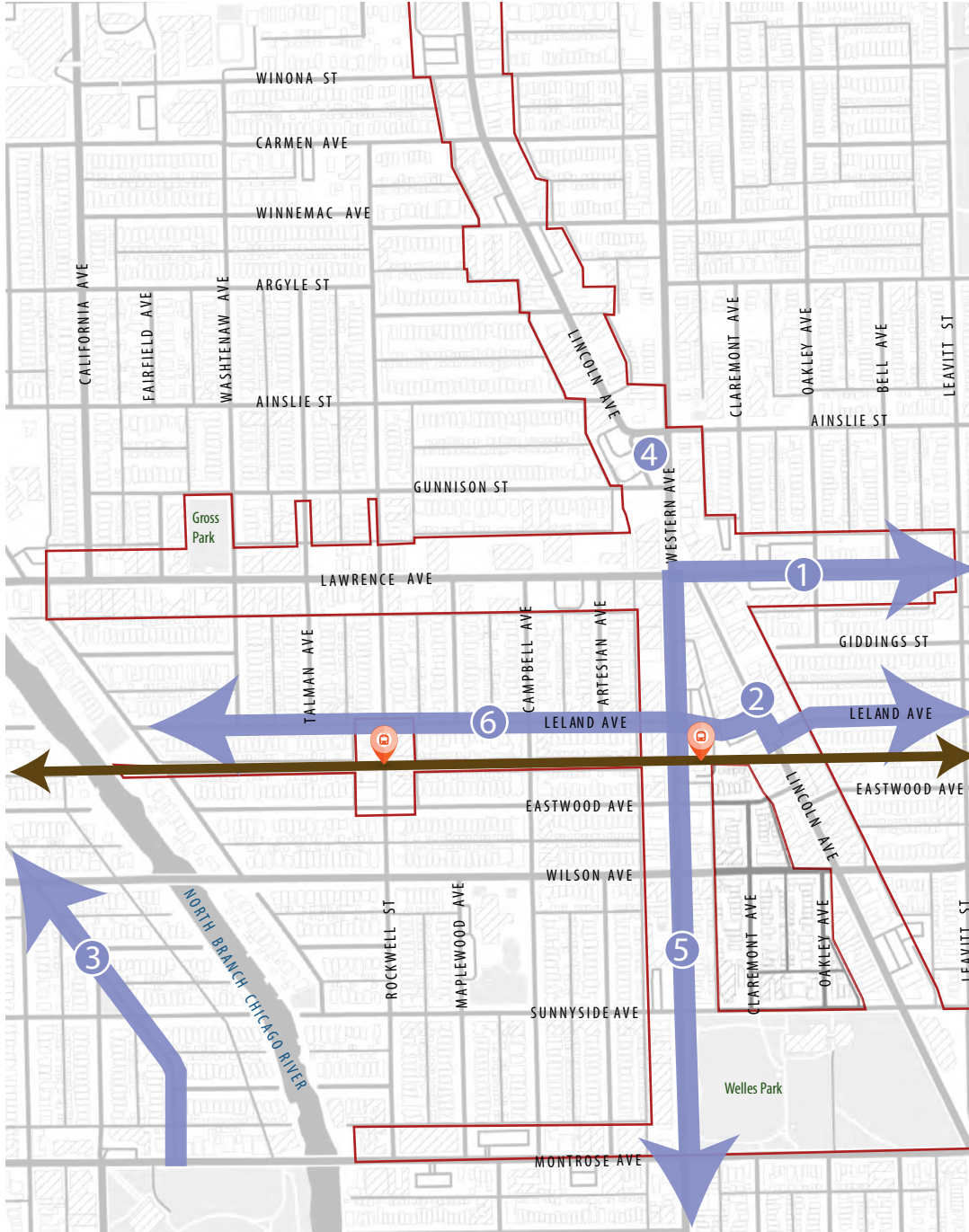


IMPROVE CONNECTIONS FOR ALL MODES

Lincoln Square is a well connected community with access to the Western and Rockwell Brown Line CTA stations, multiple CTA bus routes, (including route numbers 49, 49B, 78, 81, and 11), Divvy stations, bike routes (both existing and planned), and connected sidewalks. Yet the safety and connectivity of pedestrian and bicycle transit, in particular, must be improved to connect residents to destinations.




In 2015, the City of Chicago amended the Transit Oriented Development (TOD) Ordinance that was first enacted in 2013. The TOD Ordinance provides incentives in terms of reduced parking and minimum lot area requirements. The revised ordinance doubles the TOD eligibility zone to 1,320 feet (1/4-mile) from a transit station or 2,640 feet (1/2-mile) along a Pedestrian Designated Street (P Street), like Lincoln Avenue in Lincoln Square. Developments in these areas can propose residential parking reductions up to 100% of the normally required amount, provided they meet certain criteria. The ordinance promotes alternative transportation and encourages denser development near transit nodes that can serve residents. With its Pedestrian Streets and two CTA stations, Lincoln Square is ripe for transit oriented development.





Projects

- 1 LAWRENCE AVENUE STREETSCAPE (WESTERN AVE TO CLARK ST)
- 2 LINCOLN-LELAND CURVE
- 3 MANOR AVENUE NEIGHBORHOOD GREENWAY
- 4 LINCOLN SQUARE GATEWAY
- 5 WESTERN AVENUE VISION ZERO PL (LAWRENCE AVE TO IRVING PARK F)
- 6 LELAND AVE GREENWAY

-  Brown Line Station
-  Brown Line
-  SSA #21 Boundary



Objective 1: Improve Safety for People Walking and Biking

Safety along and across Lincoln Square's streets, sidewalks and public spaces is paramount to the neighborhood's livability. Conflicts are avoidable and should be considered unacceptable on the streets of Lincoln Square and Chicago overall. Recent initiatives by the City of Chicago and Chicago Department of Transportation (CDOT) include the Vision Zero program, which aims to improve street safety for all users. The projects identified here align with and build upon these programs.

PROJECT CHECKLIST

- 1.1: Build upon Vision Zero
- 1.2: Improve access to transit

PROJECT 1.1

Build Upon Vision Zero

Between 2013-2016, high traffic areas along Western and Lawrence Avenues saw the most car crashes involving pedestrians and cyclists. Since that time, improvements derived from the Vision Zero program, such as pedestrian refuge crossings along Western Avenue and streetscape improvements along Lawrence Avenue, have been installed to lower the number of these potentially fatal incidents.

Vision Zero's methods include:

- Collecting and using data to prioritize limited City resources towards successful and effective approaches
- Choosing effective street designs that prioritize safety
- Using enforcement to prevent the most dangerous roadway behaviors
- Working with community leaders and all Chicagoans to educate on safe roadway behaviors

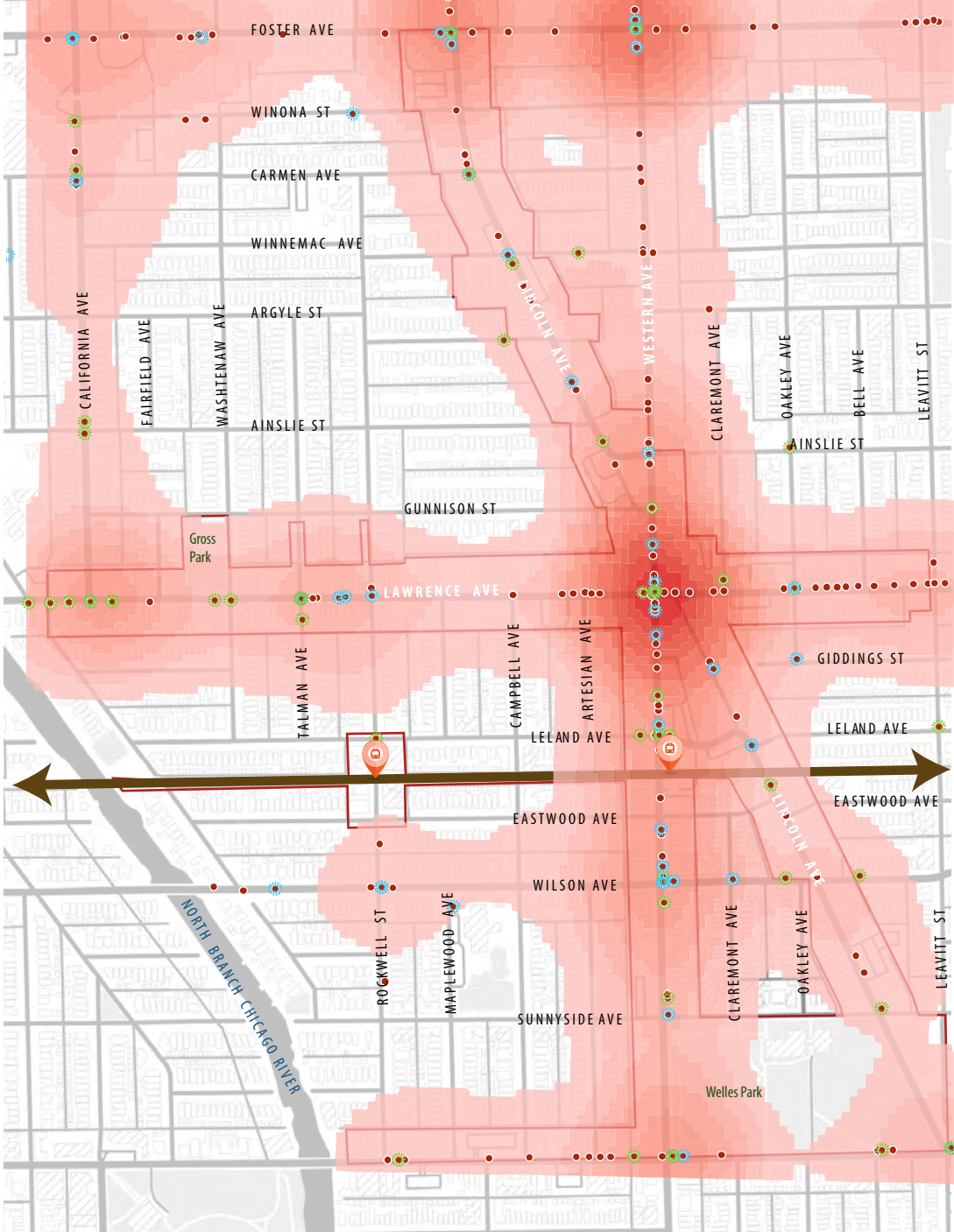
(visionzerochicago.org)

As Lincoln Square continues to partner with Aldermanic offices and CDOT on future roadway improvements, cyclists and pedestrian safety must be given special priority.

Actions

- **Improve bike and pedestrian crossings at intersections where crashes are concentrated, such as:**
 - Lawrence and California
 - Lawrence and Virginia
 - Lawrence and Talman
 - Lawrence and Rockwell
 - Lawrence and Claremont
 - Lincoln and Winnemac
 - Lincoln and Ainslie
 - Lincoln and Argyle
- **Establish a network of low stress bicycle facilities throughout the neighborhood, building off of recent and planned facilities along Lincoln Avenue and Leland Avenue**





Crash Analysis

- All Crash Types
- Crash involving Pedestrian
- Crash involving Bicyclist
- Crash Density (High to Low)

Lincoln Square Crash Data Source: IDOT (2013-2016)

- 📍 Brown Line Station
- Brown Line
- SSA #21 Boundary

PROJECT 1.2

Improve Access to Transit

For the 110,000 monthly riders at the Western Station and the 48,000 monthly riders at the Rockwell Station, the areas surrounding Lincoln Square's CTA stations serve as gateways to the neighborhood, providing access to surrounding businesses and gathering spaces for passers-by. As such, station areas are opportunities to not only address safety and access, but also to promote community development. Bus access along Lincoln Square's corridors is also critical to maintaining connectivity throughout Lincoln Square and surrounding areas.

Partnerships with CDOT and CTA are critical to addressing the areas in and around transit stations.



Actions

- Prioritize pedestrian connections and crossing enhancements at bus stops with high ridership:**
 - Western and Leland
 - Western and Lawrence
 - Western and Montrose
 - Foster and Lincoln

- Partner with CTA to improve aesthetics and access to of the station areas**




- Partner with CDOT and CTA to develop gateway signage in front of the CTA structure at Western Avenue**

- Partner with CDOT and CTA to add wayfinding signage to direct travelers from station areas to Lincoln Square businesses**

- Partner with CDOT and CTA to improve the plaza around the Western Brown Line station and beneath the Elevated structure , adding enhanced lighting, pavements, plantings, etc.**



Bus Routes

-  Brown Line Station
-  Brown Line
-  SSA #21 Boundary



PROJECT CHECKLIST

- 2.1: Evaluate east Lawrence improvements to inform the west Lawrence streetscape
- 2.2: Identify potential improvements to traffic signal timings

Objective 2: Improve Traffic Flow

Lincoln Square's roadways must balance traffic from buses, bicycles, and pedestrians with traffic from automobiles. According to data from the Illinois Department of Transportation (IDOT), average automobile traffic on these roadways is as follows:

- Lawrence Avenue: 15,700 – 17,900 vehicles per day
- Western Avenue: 16,800 vehicles per day
- Lincoln Avenue: 9,900 – 14,100 vehicles per day

Currently, bicycle facilities exist along Lawrence Avenue and Lincoln Avenue south of Leland. California Avenue, Wilson Avenue, and portions of Lincoln, Western, and Leland Avenues are identified by CDOT as Streets For Cycling Routes. Plans are currently underway to develop a neighborhood greenway along Leland Avenue from east of Lincoln Square to Rockwell Street.

Divvy bike stations are located throughout the SSA. Per Divvy records between April – June 2018, 12,600 Divvy trips were documented to and from the SSA area. The highest use of Divvy bikes was documented at the Divvy station in the Western Avenue CTA plaza.

Additionally, Lincoln Avenue between Lawrence Avenue to Sunnyside Avenue is designated as a Pedestrian Street (or P-Street). This designation is intended to preserve and enhance the character of streets and intersections in Chicago. P-Streets include regulations for building facades, entrances, parking requirements and prohibited uses. P-Streets encourage sidewalk cafes, outdoor seating and landscape plantings that promote a pedestrian first environment.

PROJECT 2.1

Evaluate East Lawrence Improvements to Inform the West Lawrence Streetscape

The Lawrence Avenue streetscape project was planned to be implemented in two phases. The recently completed Phase 1 extends between Ashland Avenue at the east to Western Avenue at the west. Phase 2 is slated to extend between Western Avenue at the east to Kedzie Avenue at the west. The streetscape utilizes widened sidewalks and dedicated bike lanes to make non automotive travel more appealing. Other features of the streetscape include improved lighting, community identifiers, site furnishings, permeable paving, stormwater bump outs and landscape plantings. As Lincoln Square plans to embark on the streetscape's second phase, key lessons should be considered moving forward, such as the need for coordination between adjacent land uses, appropriate site furnishing placements and maintenance and durability of plant materials.

Actions

- ❑ **Work with Aldermanic offices to guide site developments reflecting the pedestrian oriented nature of Lawrence Avenue**
- ❑ **Partner with CDOT to ensure bench placement is compatible with adjacent land uses and suited to pedestrian comfort**
- ❑ **Insofar as the SSA is responsible for landscape maintenance, partner with CDOT to appropriately select and install new plantings**



PROJECT 2.2

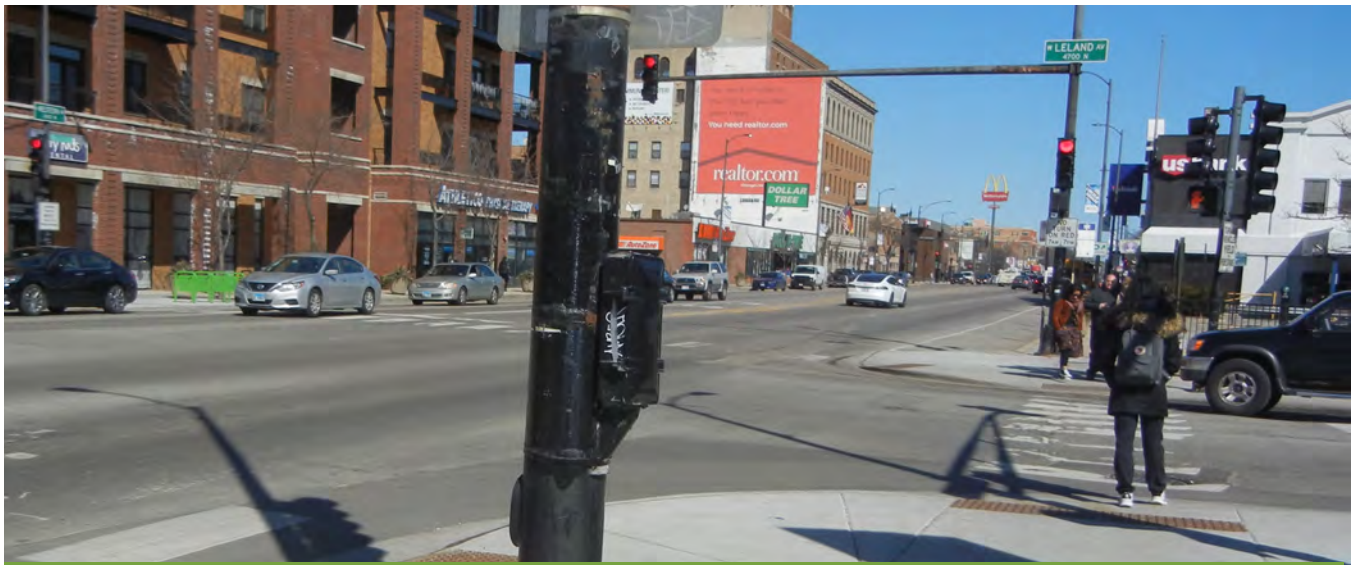
Identify Potential Improvements to Traffic Signal Timings

Reliable travel times are important to people no matter how they choose to get around. Appropriately timed traffic signals are critical to efficient traffic flow throughout the SSA. Inefficient flow can lead to congestion and frustration for drivers and can also create safety hazards if drivers speed up in order to get through a signal.

Signals with pedestrian countdown timers, where a number counts down the time remaining for pedestrians to clear a crosswalk, are more intuitive than traditional pedestrian signals. A pedestrian countdown timer begins in conjunction with the flashing “DON’T WALK” signal and allows pedestrians to decide if they have enough time to complete a crossing. All new or modernized traffic signals are required to include countdown timers, and old ones should be upgraded accordingly. All countdown timers should be programmed to allow street crossings at a maximum walking speed of 3.5 feet per second. Walking speeds slower than 3.5 feet per second should be considered at all locations, particularly at crossings typically used by children, seniors and people with disabilities.

Actions

- ❑ **Call for volunteers to take an inventory of all pedestrian signals within the SSA**
- ❑ **Identify with CDOT opportunities to upgrade pedestrian signals in conjunction with other projects**
- ❑ **Work with Aldermen to study signal timings and identify places where signals could be added, upgraded, or adjusted, focusing on these critical corridors:**
 - Western Avenue, Wilson – Lawrence
 - Lawrence Avenue, Oakley – Western
 - Montrose Avenue





Objective 3: Reinforce a Sense of Place

Lincoln Square, particularly its core surrounding Giddings Plaza, is a beloved place due in part to the walkable environment of Lincoln Avenue and the many festivals and events that come and go in this part of the neighborhood. Welcoming though it is, Lincoln Square could still be more attractive to residents and visitors alike.

PROJECT CHECKLIST

- 3.1: Maintain a special event space that complements and connects the Western Avenue Brown Line Station to businesses on Lincoln Avenue
- 3.2: Establish gateway and wayfinding signage for all modes of travel
- 3.3: Encourage pedestrian activity by creating a flexible streetscape along Lincoln Avenue

PROJECT 3.1

Maintain Special Event Space That Complements and Connects the Western Avenue Brown Line Station to Businesses on Lincoln Avenue

Actions

- Continue to program community events at the parking lot abutting the Western Brown Line station
- Develop a concept plan in coordination with CDOT and CTA to enhance the parking lot with future improvements, including access to power, enhanced surface treatments, and landscaping

Current pathway is a dismal dirty pigeon haven. Add attractive (warm) lighting, shade-loving perennial plants, cheerful paint color to train structure and art projects along the pathway. Sound dampening would be very helpful here too. As a juncture between mass transit and LS, this area serves a huge number of people every day, including tourists.

-Comment posted to the project website.

PROJECT 3.2

Establish Gateway and Wayfinding Signage for All Modes of Travel

The Square is recognized as a destination for shops and dining. However, many passers-by remain unaware of destinations throughout the SSA. Gateway and wayfinding signage can extend the visitor experience, encouraging walkers and bikers to explore Lincoln Square's different corridors. Typically gateway signs mark the entrances to neighborhoods and districts. Wayfinding signs provide direction to community destinations.

Actions

- Partner with CDOT to develop a conceptual wayfinding sign plan
- Partner with local businesses to develop a business sign kiosk that promotes their locations
- Partner with CTA and CDOT to develop gateway signs at key locations throughout the SSA

"There are some wide sidewalks here that look out of place. Could add seating, sculptures, colorful paint, above-ground planters, Little Free Library, community bulletin board... something"

-Comment posted to project website about Western Ave.



PROJECT 3.3

Encourage Pedestrian Activity by Creating a Flexible Streetscape Along Lincoln Avenue

Lincoln Avenue between Lawrence and Leland, commonly referred to as simply The Square, serves a variety of day-to-day and event specific functions. The existing roadway configuration includes diagonal parking at the east side of the street and a one-way southbound travel lane. The diagonal parking arrangement inhibits visible pedestrian crossings, and curbs and gutters become obstacles for special events that would preferably be held on level ground. A “shared street,” in which the sidewalk lies level with the roadway, might be utilized here to overcome this latter challenge. As this section of street is at the heart of Lincoln Square, home to many stakeholder residents and businesses, any modifications would require careful study. Design objectives would include accommodating parking and delivery needs for businesses, calming traffic, providing a pedestrian first environment, and supporting special events.

Actions

- ❑ Conduct a streetscape test using temporary paint, barricades, etc. Evaluate alternative roadway and parking strategies such as chicanes, bump outs, and parallel parking arrangements
- ❑ Develop a survey to understand outcomes of the test
- ❑ In partnership with CDOT, develop a concept plan based on the test and survey outcome



“Creating a gateway out of the train overpass at Western would be a good opportunity to mark the neighborhood.”

-Comment posted to project website.



Objective 4: Improve Connections

Recently completed transportation projects such as the Western Avenue Vision Zero crossing improvements, Lincoln-Leland Avenue Curve and Manor Avenue Neighborhood Greenway enhance connectivity within Lincoln Square. The planned Lawrence Avenue Streetscape extension from Western Avenue to the Chicago River will also improve pedestrian and bike connections between Lincoln Square and surrounding areas. Proposed transportation projects should build upon existing efforts to better connect Lincoln Square for both bicyclists and pedestrians.

PROJECT CHECKLIST

- 4.1: Identify site specific solutions to barriers that disconnect south and north Lincoln Avenue
- 4.2: Identify and promote low stress north-south and east-west bike routes
- 4.3: Prioritize connections to nearby bike and pedestrian facilities, both existing and planned

PROJECT 4.1**Identify Site Specific Solutions to Barriers That Disconnect South and North Lincoln Avenue**

As Lincoln Avenue travels diagonally through the Chicago grid, it is disrupted by the intersections of Gunnison Street, Western Avenue and Lawrence Avenue. Developments in these areas should be planned with an eye toward connecting the heart of Lincoln Square with neighborhoods west of Western and north of Lawrence. Plazas surrounding the Lincoln-Lawrence-Western intersection can serve as gateways, creating physical and visual continuity between the separate sides of these major thoroughfares.

Actions

- Enhance Lincoln Ave with gateway enhancements at the Lincoln Statue Plaza and the triangular green space adjacent to McDonalds
- Partner with CDOT to improve signal timing at key intersections with Lincoln Avenue
- Partner with CDOT to improve Lincoln Avenue's pedestrian crossings with striping and countdown timers

“Revitalize Western Ave: It would be great to find ways to turn this major neighborhood artery into a welcoming, pedestrian-friendly thoroughfare. Filling the large number of vacant storefronts would be a great start. And protected bike lanes would also encourage destination commerce, in addition to creating an overall calming effect.”

-Comment posted to project website.

PROJECT 4.2**Identify and Promote Low Stress North-South and East-West Bike Routes**

Although bike routes are designated along Lincoln Avenue, Western Avenue and Lawrence Avenue, additional low stress routes should act as alternatives to these busy streets. The Manor Avenue Neighborhood Greenway is an example of a low stress bike route that connects the neighborhood along the west side of the Chicago River to the Chicago River Trail. Similarly, Leland Avenue in Lincoln Square is being converted into a low stress east-west connector. A similar north-south connector should be evaluated for Lincoln Square, perhaps along Rockwell Street and/or Leavitt Street.

Actions

- Conduct neighborhood bike rides and follow up surveys regarding proposed low stress bike routes to understand demand and build support
- Work with local businesses and bicycling advocates to promote low stress bike routes
- Partner with CDOT to develop plans for low stress bike routes, marked with formal signage and equipped with appropriate traffic calming devices

PROJECT 4.3

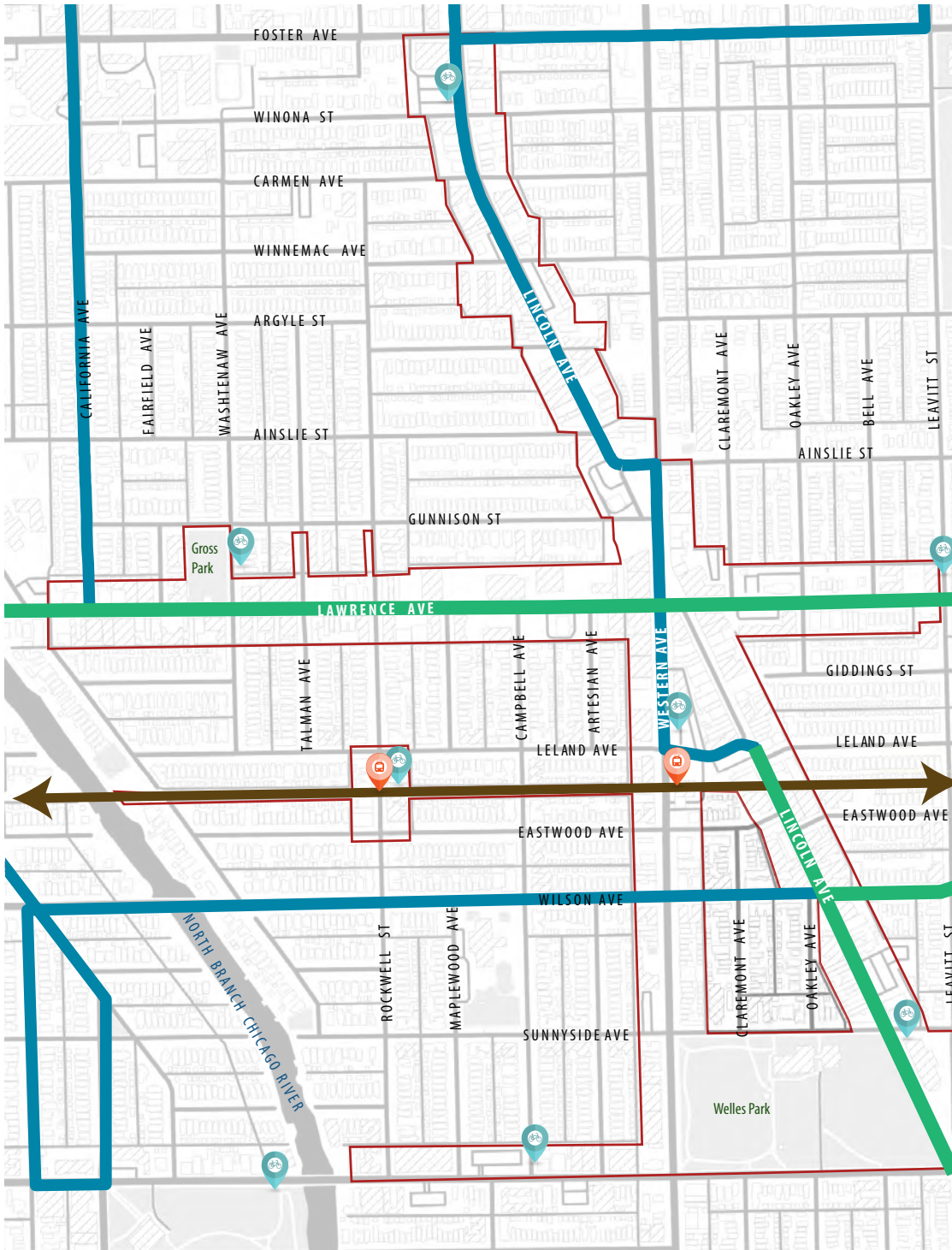
Prioritize Connections to Nearby Bike and Pedestrian Facilities, Both Existing and Planned

Chicago is home to a large network of designated bike routes, including Lawrence Avenue and Lincoln Avenue south of Leland Avenue. In addition, Foster Avenue, California Avenue, Wilson Avenue, and Lincoln Avenue north of Leland Avenue are all CDOT designated Streets for Cycling. As the SSA plans bike related improvements, such as bike route expansions, low stress routes and new and upgraded bike facilities, these improvements should be prioritized. Such a strategy would support, rather than compete with, CDOT's efforts to make Chicago more bike friendly.

Actions

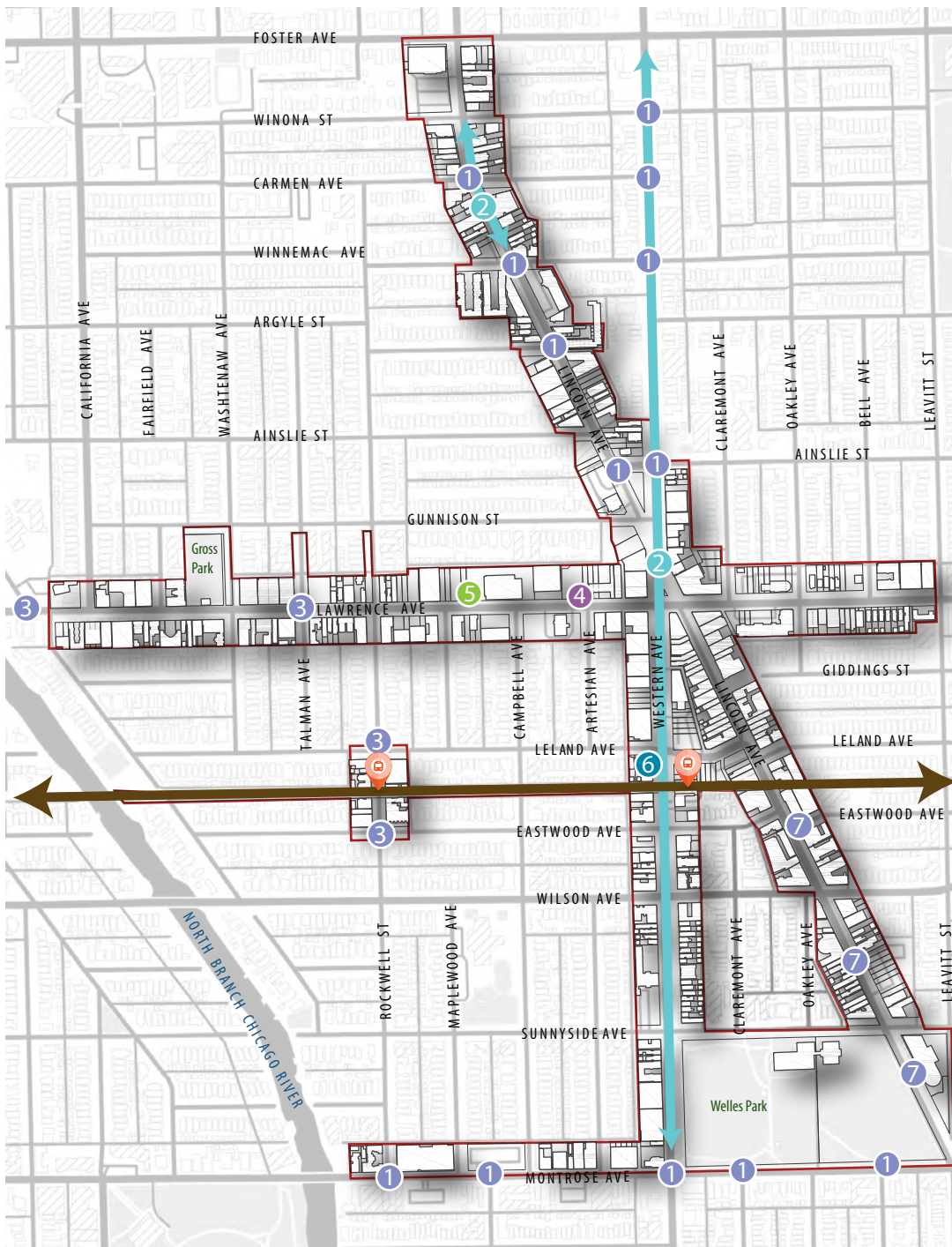
- ❑ Monitor Divvy bike uses to understand ridership trends
- ❑ Continue planning the Leland Avenue Greenway and potential north-south low stress bike routes
- ❑ Partner with CDOT to strategically install bike racks and other bike facilities, such as repair stations, as appropriate








Bicycle Facilities

- Existing Bike Facility
- CDOT Street's for Cycling Route
- Divvy Station
- Brown Line Station
- Brown Line
- SSA #21 Boundary



Master Plan Recommendations

- 1 BUMP OUT / IMPROVED CROSSWALK
- 2 BOULEVARD/ REFUGE ISLAND
- 3 CROSSING ENHANCEMENT
- 4 REDUCE CURB CUTS
- 5 SIDEWALK/ PARKING LANDSCAPING
- 6 IMPROVED BROWN LINE STATION ACCESS
- 7 NEW CROSSWALK

-  Brown Line Station
-  Brown Line
-  SSA #21 Boundary



CHAPTER 3 | PROGRESS

SUPPORT LOCAL BUSINESS +
GUIDE NEW DEVELOPMENT



CHAPTER 3 PROGRESS

Objective 1:
Preserve character

Objective 2:
Support local businesses

Objective 3:
Help to fill vacancies

Objective 4:
Encourage a range of residential

Objective 5:
Promote quality development that
reflects community vision and
corridor character



SUPPORT LOCAL BUSINESSES + GUIDE NEW DEVELOPMENT

Lincoln Square is recognized throughout the Chicagoland area as a unique neighborhood, filled with locally owned shops, vibrant community events, and walkable shopping and dining areas. While The Square is widely recognized as a destination for shopping, dining and gathering, the surrounding corridors also offer a range of community destinations. As these corridors continue to evolve, the characteristics of new development should further enhance walkability, local character, diversity and connection to the cultural richness of Lincoln Square.





Objective 1: Preserve Character

Throughout the planning process, it was clear residents wanted to protect the unique character and identity of The Square: the portion of Lincoln Avenue between Lawrence and Leland Avenues that has the feel of a special “room”. This area is home to many local businesses, pedestrian friendly streets, and the iconic Giddings Plaza. In addition to The Square, other corridors within the SSA’s boundaries offer a mix of characters and uses. These corridors include Rockwell Crossing, Lincoln Avenue south of Lawrence, and portions of Western and Lawrence Avenues. This plan seeks to preserve the existing positive characteristics of these corridors and strengthen their connections to one another.

PROJECT CHECKLIST

- 1.1 Support events programming
- 1.2 Market local businesses with institutions
- 1.3 Improve access and wayfinding

“Our Events Rock! : The community events in Lincoln Square are so wonderful! Fun for all ages and they really bring residents together. Anything we can do to further community cohesion while supporting our local businesses is a win in my book.”

-Comment posted to project website.

PROJECT 1.1

Support Events Programming

The SSA’s service provider, the LSRCC, currently manages approximately 14 events per year. Special events such as the Summer Concert Series, Square Roots Festival and Apple Fest attract visitors from the community, Chicagoland and beyond. These events should be supported by any infrastructural and placemaking projects taking place along the Lincoln corridor. Amenities like power, lighting, and special signage should all be considered.

Actions

- Continue working with local businesses to coordinate festivals and events in public areas
- Continue to create attractive and engaging promotional materials for special events
- Partner with businesses in Rockwell Crossing to expand events and fests

PROJECT 1.2

Market Local Businesses With Institutions

Lincoln Square’s institutional partners provide cultural richness and diversity to the community. Institutions such as the Old Town School of Folk Music, DANK Haus German American Cultural Center, and Cambodian Association of Illinois provide venues and services that draw visitors into the neighborhood and out into local businesses. Lincoln Square’s largest institutions depend in turn upon the neighborhood’s smallest businesses. This positive relationship should be made stronger by initiatives moving forward.

Actions

- Continue to promote local businesses on the Lincoln Square Ravenswood Chamber of Commerce website and social media
- Encourage greater partnerships between anchor institutions and local businesses



Image Credit: Lincoln Square Ravenswood Chamber of Commerce

PROJECT 1.3

Improve Access and Wayfinding

Businesses in the SSA must be both visible and accessible in order to thrive. Beyond physical improvements like wayfinding signage, the SSA should work with the LSRCC to give local businesses a greater digital presence.

Actions

- ❑ Develop wayfinding signage to direct visitors to municipal destinations and cultural institutions
- ❑ Erect business directory kiosks that advertise local businesses to pedestrians
- ❑ Establish a blade signage program, through which businesses can design, fabricate and install blade signs in tandem with one another
- ❑ Assist local businesses with social media and website development





Objective 2: Support Local Businesses

Much as local residents love local businesses, national chains and online retailers pose constant challenges to small shops' bottom lines. As Lincoln Square has grown, franchises and national brands have moved to the area to take advantage of increased activity and rising incomes. National stores and businesses can coexist with local businesses, however, particularly along Western Avenue and portions of Lawrence Avenue where they can lease larger storefronts and traffic counts are highest.

Steps can be taken to protect the mom-and-pops of Lincoln Square and invite more small businesses into the area. The dedicated efforts of the LSRCC, SSA, anchor institutions and residential groups are necessary to provide these small businesses with regular foot traffic. Further partnership with Aldermanic offices and commercial property owners can create hospitable zoning and building enhancements catered to small businesses.

PROJECT CHECKLIST

- 2.1 Encourage developers to lease to local businesses
- 2.2 Coordinate City/County incentives for small businesses
- 2.3 Continue and expand local events to market local businesses
- 2.4 Create a Pop up program

PROJECT 2.1**Encourage Developers to Lease to Local Businesses**

Rising rents and expenses make it increasingly difficult for local businesses to stay profitable. Developers and property owners in Lincoln Square should be encouraged to reserve appropriate spaces for local businesses to help maintain the shopping environment that is so special.

Actions

- Provide financial support for facade and building improvements through Small Business Improvement Fund (SBIF) and the SSA's facade rebate program
- Continue to develop marketing campaigns that promote local businesses in Lincoln Square
- Coordinate with developers and property owners to create accommodating commercial spaces for small and local businesses
- Host events and programs wherein local businesses can network, plan joint marketing efforts, collaborate on common issues (such as loading zones) and make connections that can benefit business practices

PROJECT 2.2**Coordinate City/County Incentives for Small Businesses**

Many programs can be leveraged to assist small businesses in Lincoln Square. However, because businesses often lack the time or expertise to navigate these programs, the Chamber, SSA and Alderman must be well equipped to assist them.

Actions

- Continue to provide outreach and assistance to local businesses
- Coordinate with the Aldermanic offices to provide streamlined services to local businesses
- Assist businesses that need help understanding and accessing beneficial programs
- Communicate opportunities and deadlines to local businesses via the LSRC website and social media



PROJECT 2.3

Continue and Expand Local Events to Market Local Businesses

Community fests and events are a tremendous draw for Lincoln Square, attracting tens of thousands of visitors throughout the year. These events are opportunities to market local businesses and encourage visitors to return to the neighborhood.

Actions

- ❑ **Conduct pre and post event outreach to understand how businesses are impacted and how to effectively market them**
- ❑ **Expand local events to other interested SSA areas, such as Rockwell Crossing**
- ❑ **Create marketing campaigns that cross promote local businesses, encouraging visitors to extend their stays**
- ❑ **Work with businesses to develop promotions that encourage visitors to shop after festivals**



Image credit: Jordan Ziolkowski

PROJECT 2.4

Create a Pop Up Program

Pop up programs are effective means of promoting local businesses and encouraging entrepreneurship. Home based and other small businesses can apply for pop up licenses and take up temporary residencies in Lincoln Square's empty storefronts. This can be particularly effective during the winter holiday season when sales are at their highest. Funding assistance, joint marketing campaigns, and collaborative efforts between property owners can provide the pop up shops the support they need to afford overhead costs and open for business.

Actions

- Develop a business plan for interested pilot pop up businesses in Lincoln Square, and help them activate a vacant storefront**
- Determine necessary costs for storefront improvements, staffing, marketing, insurance and related costs**
- Secure vacant space to launch the program, securing at least a three-month commitment from a property owner**
- Launch a pilot project for the pop up program**
- If successful, determine the business model**





Objective 3: Help to Fill Vacancies

At the time of this Master Plan assignment, the SSA had 1.2 million square feet of commercial space, of which 4.3% was listed as vacant, according to CoStar, a supplier of commercial real estate information. The larger market area, bounded by Bryn Mawr Avenue and Peterson Avenue at the north, Irving Park Avenue at the south, Ravenswood Avenue at the east and the Chicago River at the west, had more than 3.2 million square feet of commercial space (including a new 70,000 square foot Mariano's on the former Sears parking lot on Lawrence Avenue) with a vacancy rate of only 2.6%. Average net rents in the SSA were \$20.37, or approximately \$23.93 in the market area. Because even these limited vacancies can stifle business growth in Lincoln Square, the SSA must work with its partners to ensure rent are affordable and vacant spaces are made ready for new tenants.

PROJECT CHECKLIST

- 3.1 Marketing materials
- 3.2 Renovate and redevelop older spaces
- 3.3 Encourage ground floor commercial at key development sites
- 3.4 Support non-retail uses as appropriate

PROJECT 3.1**Marketing Materials**

Marketing materials should identify available vacant spaces, organized by corridor, and highlight the positive elements of each corridor, such as transit access, cultural institutions, neighborhood demographics, surrounding businesses and other community destinations.

Actions

- Expand upon the existing vacancy database to identify available square footage and available amenities
- Create and regularly update marketing materials that describe each corridor and available vacancies
- Distribute marketing materials throughout the SSA and to other targeted groups
- Coordinate events and marketing efforts tailored to attract businesses that fit each corridor's specific needs

PROJECT 3.2**Renovate and Redevelop Older Spaces**

Community feedback strongly reflects the public's desire to protect and maintain older buildings, which contribute to the physical character and sense of place unique to Lincoln Square. Renovating older buildings, rather than demolishing them and building new, typically helps to keep costs down for future tenants and their neighbors.

Actions

- Host renovation workshops in which property owners, businesses and local contractors can discuss opportunities for redevelopment
- Continue to market the SSA facade rebate program
- Consider expanding the facade rebate program to include interior renovations and storefront displays



PROJECT 3.3

Encourage Ground Floor Commercial at Key Development Sites

For the most part, Lincoln Square is blessed with seamless, active commercial corridors that encourage strolling. In other areas, however, lone businesses await neighbors to join them. Nomadic Ant on Western Avenue and Groundswell Coffee Roasters on north Lincoln Avenue, for example, are outposts that would be well bolstered by the presence of other, nearby businesses. When new developments are considered, commercial spaces should be proposed at the ground floor, particularly in areas where existing businesses need support, as well as highly visible intersections and areas with wide sidewalks suitable for cafe seating. Where appropriate, ground floor commercial should include 'black iron' infrastructure to accommodate future food and beverage uses.

Actions

- ❑ Distribute design guidelines to Aldermanic offices
- ❑ Host meetings with prospective developers to discuss community preferences and design guidelines pertinent to their proposed sites
- ❑ Introduce prospective developers to local businesses who wish to rent commercial space

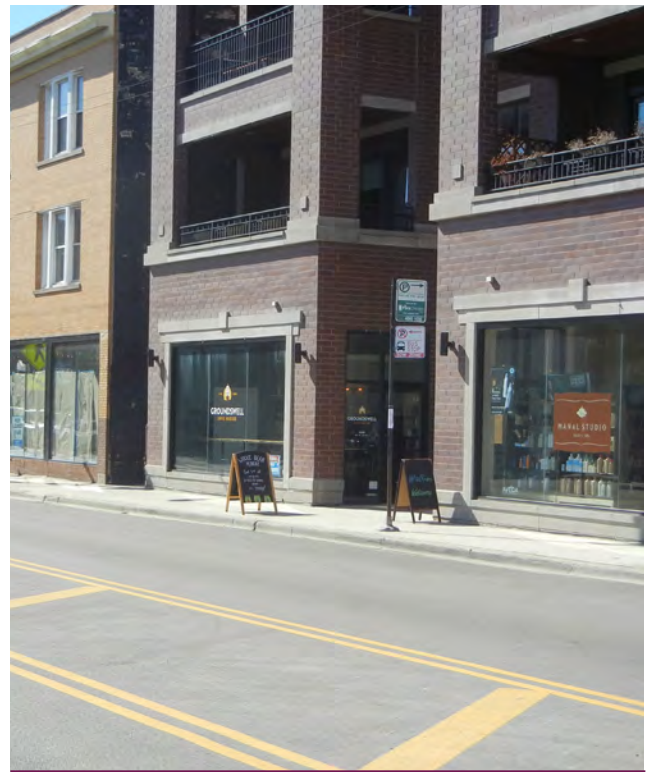
PROJECT 3.4

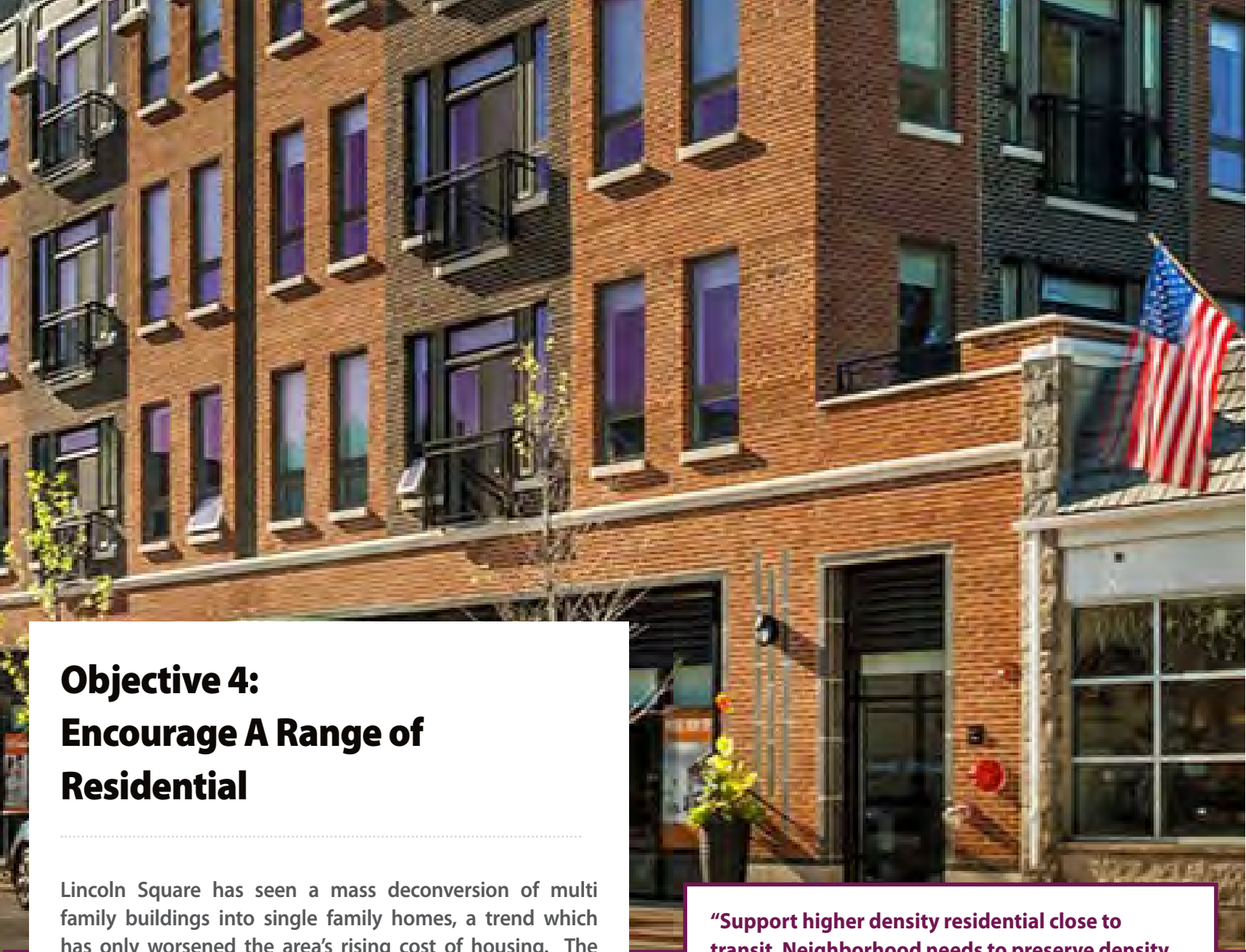
Support Non-Retail Uses as Appropriate

A walkable and attractive corridor environment should not be limited to retail or restaurant uses only. When designed properly, ground floor spaces can support other non retail uses, such as personal and professional services, fitness uses, offices, and gallery spaces. Appropriate design considerations such as storefront transparency, signage, lighting and other amenities can minimize vacancies and add new character and activity to corridors in need.

Actions

- ❑ Market first floor spaces as opportunities for services and office uses
- ❑ Introduce property owners and developers to organizations who wish to rent commercial space
- ❑ Host meetings with property owners and local businesses to review design guidelines pertaining to first floor storefronts





Objective 4: Encourage A Range of Residential

Lincoln Square has seen a mass deconversion of multi family buildings into single family homes, a trend which has only worsened the area's rising cost of housing. The neighborhood's convenient transit, walkable shops and dining, cultural institutions, great schools and beautiful parks, have substantially increased property values. The median home value is \$420,000 in the SSA, a price too expensive for many younger homeowners, artists, seniors and others. The community has embraced TOD to attract additional housing and density while sacrificing space for parking. TOD, among other strategies, should be used to provide a range of affordable housing types and densities.

"Support higher density residential close to transit. Neighborhood needs to preserve density to support shops and diversity of age and income. Neighborhood is actually losing density (per the census) Retail can only survive if there are enough people to shop in the shops. The more people who live close to shops the more people who can walk and bike to get their daily needs. The more people who live close to transit, the less they need cars to get to work. All of these lead to decreased dependency on automobiles and a lower cost of living. That being said - density does not by default equal affordability, that is more complex. But less density does lead to less affordability for sure. Ideally, there should be a robust mix of housing sizes and densities."

-Comment posted to the project website.

PROJECT CHECKLIST

- 4.1 Increase housing type and variety
- 4.2 Encourage transit oriented development that includes residential density

PROJECT 4.1
Increase Housing Type and Variety

A variety of factors have led many longtime residents to feel priced out of Lincoln Square. For local seniors aging out of their homes, few affordable options exist within the neighborhood. As a result, seniors are especially vulnerable to rising housing costs. New housing should provide space for seniors, as well as other groups that require unique and affordable housing types.

Actions

- Work with developers and Aldermen to include affordable units in new market-rate, mixed-use developments, and encourage senior housing where appropriate
- Identify older buildings that could be renovated to include affordable rental units
- Identify incentives such as Historic Tax Credits and Housing Tax Credits to help subsidize the costs of renovating older buildings

PROJECT 4.2
Encourage Transit Oriented Development that Includes Residential Density

Lincoln Square benefits from two CTA transit stations at Western Avenue and Rockwell Crossing. These and the Lincoln Avenue Pedestrian Street could all be used as centers for TOD. Clustering housing near transit nodes puts more people in the middle of the neighborhood’s most active areas, providing nearby businesses with as many potential customers as possible.

Actions

- Promote a diverse range of housing options that include TOD
- Provide information to new residents to connect them with community resources



Objective 5: Promote Quality Development That Reflects Community Vision and Corridor Character.

Western Avenue is different than Lincoln Avenue, Rockwell Crossing is different than The Square and Lawrence Avenue east of western is different than its counterpart west of Western. While the SSA should make every effort to tie Lincoln Square together, it should also lean into the things that make each individual corridor special.

A primary goal of this Master Plan was the creation of design guidelines, which could be used to inform future development on a street-by-street basis. The purpose of these guidelines is not to standardize the appearance of buildings, or to limit creativity, but rather to reflect the community's goals, values and aesthetic preferences as they related to their built environment. These guidelines can assist the preservation of older buildings, as well as the redevelopment and construction of newer ones. Furthermore, these guidelines must be followed in addition to local ordinances and applicable building codes. They are intended to supplement guidelines already imposed by the City of Chicago and other governing bodies.

The guidelines that follow have been drafted to assist the development community, neighborhood organizations, Aldermanic offices and public agencies review specific proposals and create long term strategies for improving the commercial corridors of Lincoln Square.



“Development on Lincoln north of Lawrence/ Western: We were also really surprised by the lack of development on that stretch of Lincoln but also excited about becoming civically engaged with our neighbors to work on that issue. It’s so close to public transportation and that thriving area on Lawrence--I see no reason why we can’t work together to improve sustainable development on that stretch of Lincoln. I am excited to hear/learn from others who have lived in the neighborhood longer and understand these issues more deeply. Preserving residential density seems important. I intend on being very engaged around this and excited to meet others who feel the same!”

-Comment posted to the project website.

A SUMMARY OF COMMUNITY VISION

During initial community outreach, the following themes emerged regarding Lincoln Square’s physical environment:

□ Lincoln Square is a **historic, “Chicago” neighborhood**. These roots must be reinforced in the face of new development. The community indicated a preference for adaptive retrofits and contextual masonry buildings over modern steel and glass structures.

□ Lincoln Square is a community of **friendly gathering spaces**. Landscaped patios and sidewalk plazas nod to the neighborhood’s European feel. Pedestrian scaled features, such as building signage, awnings, lighting, entry details and landscaping also contribute to a hospitable environment.

□ Lincoln Square has stories to tell. **Public art and murals** express the neighborhood’s identity and support the local arts community.



Quick Poll #3 delved into building design and character further, yielding the following themes from the community:

- **Lincoln Square is a historic, Chicago neighborhood; these roots must be reinforced and preserved in the face of new development**
- **Modern buildings with extensive glass were generally poorly rated. Participants noted these buildings can be attractive but are not the right fit for Lincoln Square**
- **Masonry buildings that blend in scale with their surrounding structures are preferred**
- **Adaptive retrofits, renovations, and historic preservation of architecturally significant buildings was generally encouraged**
- **Signage and awnings need to be cohesive, interesting and attractive**
- **Streetscape should integrate nature including a balance of greenery**
- **Sidewalk patios and pedestrian plazas are encouraged, but should not hinder pedestrian mobility**
- **Building height to setback ratios should be reflect neighborhood context and support pedestrian comfort**
- **Ground level details, such as awnings, lighting, entryways, plazas and greenery are important**
- **Public art and murals should be considered at key locations and blank facades**

Based on the comments received during outreach, building development design guidelines are organized into two sections:

1. Overall best practices
2. Corridor specific design guidelines

OVERALL BEST PRACTICES

BUILDING ORIENTATION AND SITE LAYOUT

- Buildings should be oriented to the street and sidewalk
- Off street parking should be located behind buildings, accessed from alleyways or abutting side streets. Curb cuts are discouraged along main corridors.
- Building massing should relate to the surrounding context and incorporate insets or bays wherever possible
- Perimeter buffering should be provided between new developments and adjacent residential neighborhoods (i.e. fencing, landscaping)
- Building materials should err toward traditional masonry with storefront transparency
- Building facades should include articulated cornice lines, fabric awnings, blade signage and building mounted gooseneck lighting as appropriate
- Retail and restaurants are encouraged in ground floor storefronts, especially at intersection areas
- Commercial office storefronts should incorporate semi-transparent window treatments, such as interior fabric curtains
- LED signage, flashing lighting and internally lit awnings should be discouraged

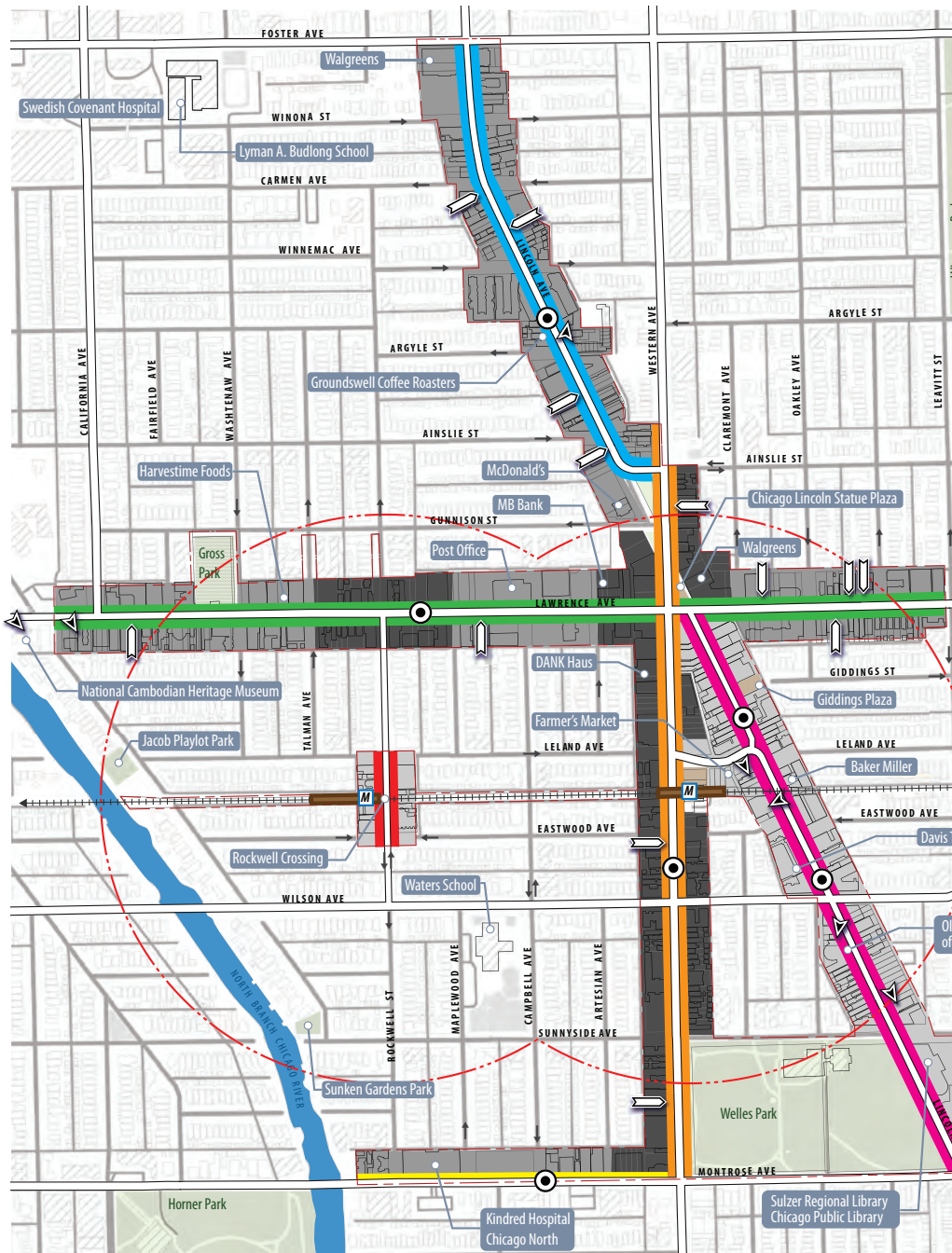
STREETSCAPE AND SIDEWALK ORGANIZATION

- Sidewalks should provide safe, clear and open access. A 6'-0" minimum width sidewalk is preferred
- Encroachments onto the public way should accommodate a 6'-0" wide passable sidewalk. When adjacent to street utilities, such as light poles and signs, the passable sidewalk width should not be less than 4'-0", per the Americans with Disability Act
- Landscape plantings should maximize views. Trees should be limbed up to 6'-0". Shrubs and perennials should not exceed 3'-0" at maturity.

AMENITIES AND SIDEWALK ACTIVATION

- Active building uses, such as amenities spaces, entrances and plaza spaces, should be visible and accessible from the sidewalk wherever possible
- Blank walls along the first-floor frontages should be discouraged
- Outdoor seating and dining areas should be installed wherever possible, especially at intersection areas
- Bike racks should be incorporated as appropriate
- Public art and murals should be incorporated as appropriate

Private Development Design Guidelines Framework Map



CORRIDOR DESIGN GUIDELINES + DEVELOPMENT

DENSITY

(Light Gray)	PRESERVE EXISTING LOW DENSITY CHARACTER
(Medium Gray)	MEDIUM DENSITY
(Dark Gray)	HIGHER DENSITY

CORRIDOR GUIDELINES KEY

(Blue)	LINCOLN AVE- NORTH
(Pink)	LINCOLN AVE- CENTRAL + SOUTH
(Green)	LAWRENCE AVE
(Red)	ROCKWELL CROSSING
(Orange)	WESTERN AVE
(Yellow)	MONTROSE AVE

⊙ CONCEPTUAL STREET SECTION LOCATION

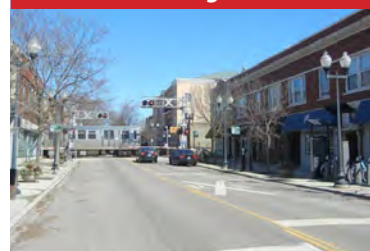
MAP LEGEND:

(Arrow)	EXISTING MURAL
(Arrow)	NEW & RECENT CONSTRUCTION
(Red Dashed Line)	SSA #21
(Green Area)	Parks and Green Space
(Brown Area)	Plaza
(M Logo)	Brown Line Station

Lincoln Ave- North



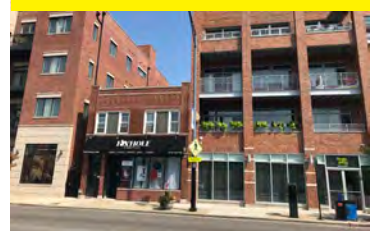
Rockwell Crossing



Lincoln Ave- South



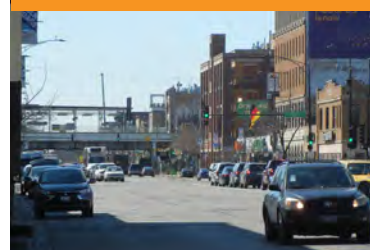
Montrose Ave



Lawrence Ave



Western Ave



CORRIDOR SPECIFIC DESIGN GUIDELINES

THE SQUARE AND SOUTH LINCOLN AVENUE

A WALKABLE, EUROPEAN-STYLE STREET WHERE NEIGHBORS SHOP, DINE, GATHER AND ATTEND SPECIAL EVENTS

Lincoln Avenue is probably the most pedestrian friendly, walkable corridor in the SSA. The relatively low scale of its 1-3 story masonry buildings, traditional storefronts, relatively wide landscaped sidewalks and narrow roadways forms an attractive outdoor “room”. Here neighbors assemble for summer concerts in Giddings Plaza, as well as farmers markets and the community’s largest street festivals. Local shops, restaurants and sidewalk cafes activate and enliven the street. More cafes and outdoor seating areas would be welcome in this area, provided they do not interfere with safe sidewalk passage.

What we heard

The Square and South Lincoln Avenue are treasured spaces in the community. Developers should employ an “if it ain’t broke, don’t fix it” strategy in this area, mirroring existing building scale and mass, and building with pedestrians in mind.

When asked: ‘What do you like most about Lincoln Square?’

79% of respondents reported ‘walkable environment’

71% of respondents reported ‘Quality of local shops, restaurants, and businesses’

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Characteristics of the existing street should be protected and maintained, including building scale and orientation to the sidewalk

Existing building facades should be maintained, including storefront windows, expressed cornice lines, building signage, landscaping and seating

Ground floor commercial uses should be encouraged to further activate to the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

□ STREETScape + SIDEWALK ORGANIZATION

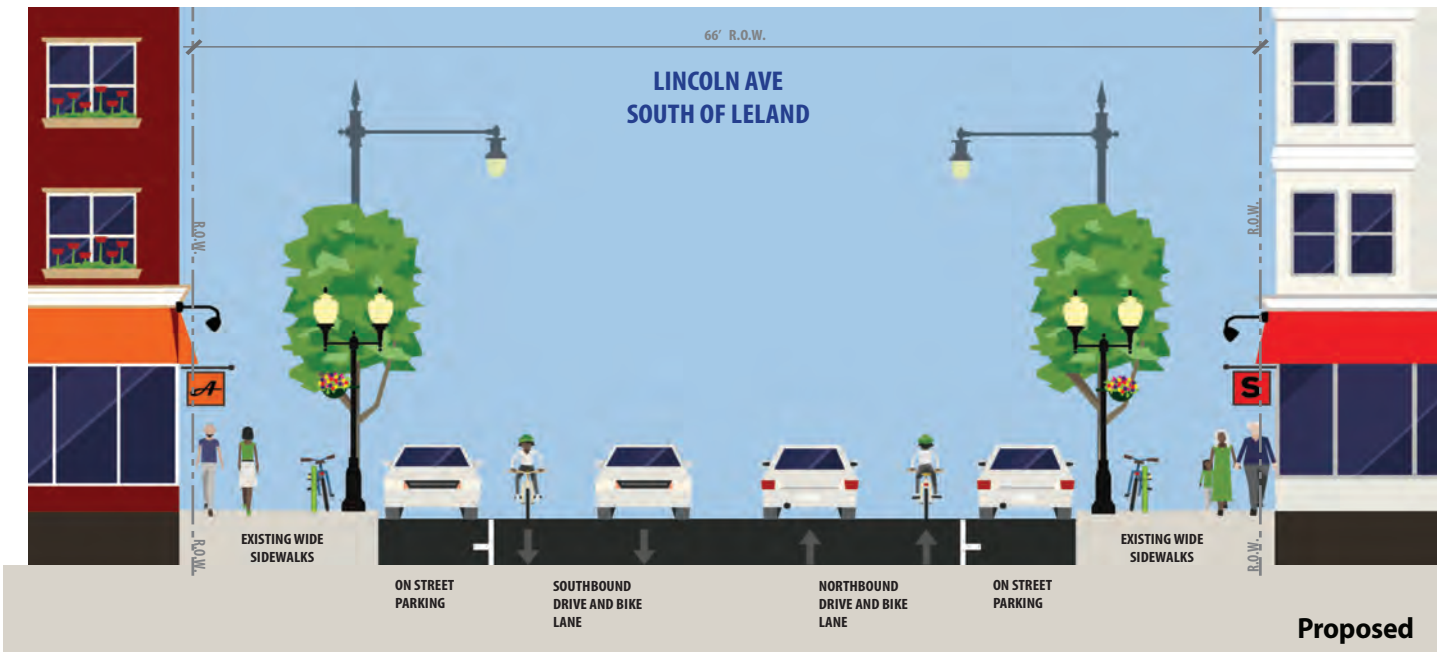
Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

Views should remain clear and open. Plants along sidewalks should not exceed 3’-0” height

□ AMENITIES + SIDEWALK ACTIVATION

Spaces for outdoor activities should complement adjacent buildings. Features such as interactive art, seating and games are encouraged to activate the sidewalk areas.



NORTH LINCOLN AVENUE

*A REDEVELOPING, MIXED USE CORRIDOR
SURROUNDED BY HIGH QUALITY NEIGHBORHOODS*

In contrast with Lincoln Avenue to the south, north Lincoln Avenue includes relatively narrow sidewalks which lack spaces for gathering, landscaping, or streetscape amenities. Building forms are varied, including 1-5 story mixed use buildings with a variety of orientations, located against the sidewalk or behind parking areas.

What we heard

61% of respondents hoped for commercial development and rejuvenation north of Lawrence Avenue. While this area is ripe for redevelopment, builders should respect the community's preferences and consider these design guidelines

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Softened building enhancements should be considered to offset the narrow street and sidewalk, including 5'-0" wide setbacks beginning above the second floor. These setbacks could house amenities and balcony spaces for residential units above.

Existing building facades should be maintained, including storefront windows, expressed cornice lines, building signage, landscaping and seating

Blade signage should be deployed to enliven the streetscape where sidewalk level enhancements are not possible

Ground floor commercial uses should be encouraged, primarily at intersections, to activate to the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

□ STREETScape + SIDEWALK ORGANIZATION

Corner plaza spaces should be created near intersections to complement commercial spaces and bring new activity to the street

Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

Views should remain clear and open. Plants along sidewalks should not exceed 3'-0" in height.

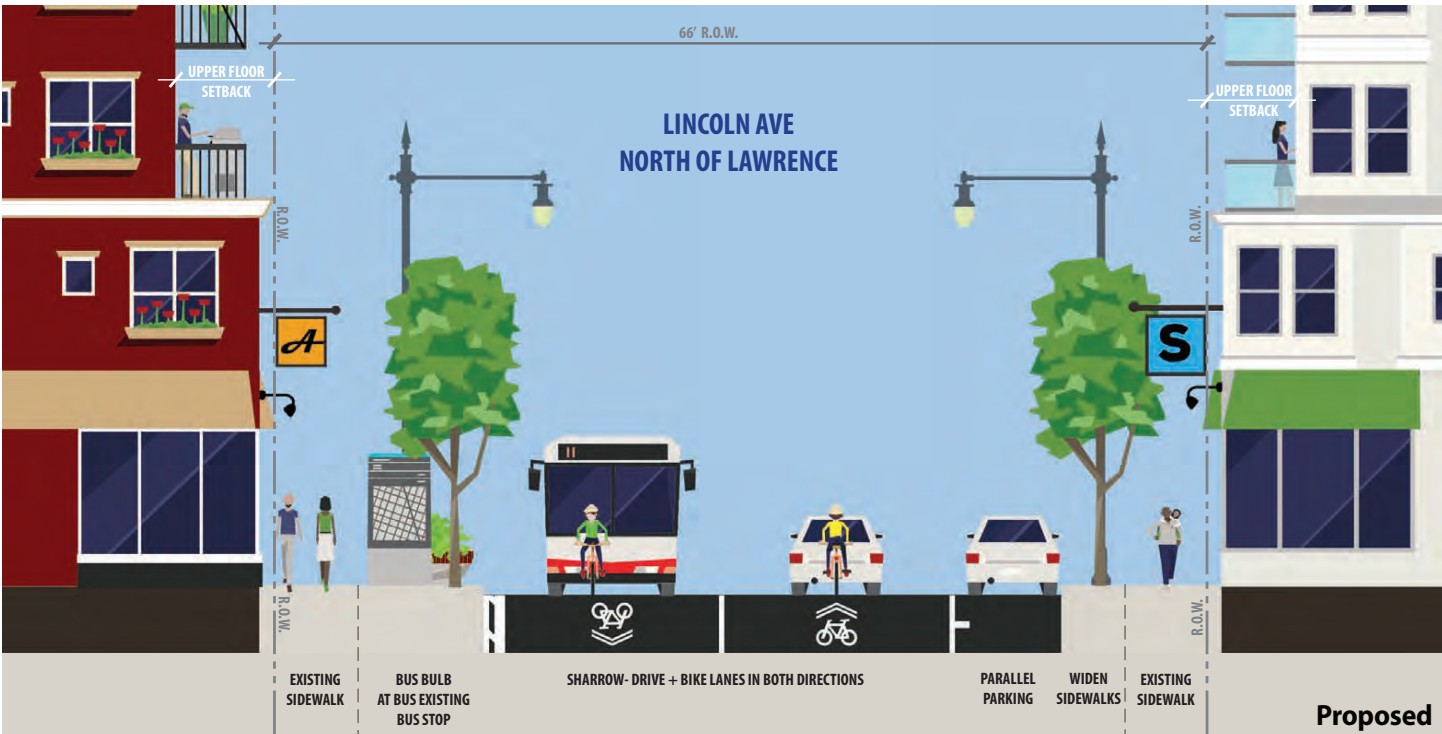
□ AMENITIES + SIDEWALK ACTIVATION

Roll up windows, which open indoor spaces up to the outside street, should be encouraged wherever possible

Parklets should be encouraged where adjacent land uses can support them with activity and maintenance



Existing



Proposed



LAWRENCE AVENUE

A DEVELOPING MIXED USE CORRIDOR WITH OPPORTUNITIES FOR GREATER PEDESTRIAN AND BIKE MOBILITY

Lawrence Avenue is undergoing significant changes. A recent streetscape renovation east of Western Avenue includes bike lanes, rain gardens, site furnishings and defined on-street parking. Public investment has correlated strongly with new development in this area. However, the mix of 1-5 story buildings along this corridor and a variety of building forms and setbacks, including parking areas along the sidewalk, create an inconsistent edge and undefined street wall. As the streetscape proceeds to the Chicago River, developments should support it with consistent building orientations and treatments that add character to the street.

What we heard

20% of respondents called for higher density, particularly near transit.

Numerous comments specifically addressed the need for more affordable housing.

Respondents called on developers to anticipate greater pedestrian activity on the parts of Lawrence Avenue receiving streetscape upgrades, and to construct their buildings accordingly.

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Curb cuts should be kept to a minimum, and parking should be moved to the rears of buildings where necessary

5'-0" wide lot line setbacks should be encouraged to maximize sidewalk and outdoor seating spaces

5'-0" wide setbacks should be encouraged for all floors above the second floor to reduce the "canyonization" effect

Ground floor commercial uses should be encouraged to activate to the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

New construction should be dense and include a variety of housing types

□ STREETScape + SIDEWALK ORGANIZATION

Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

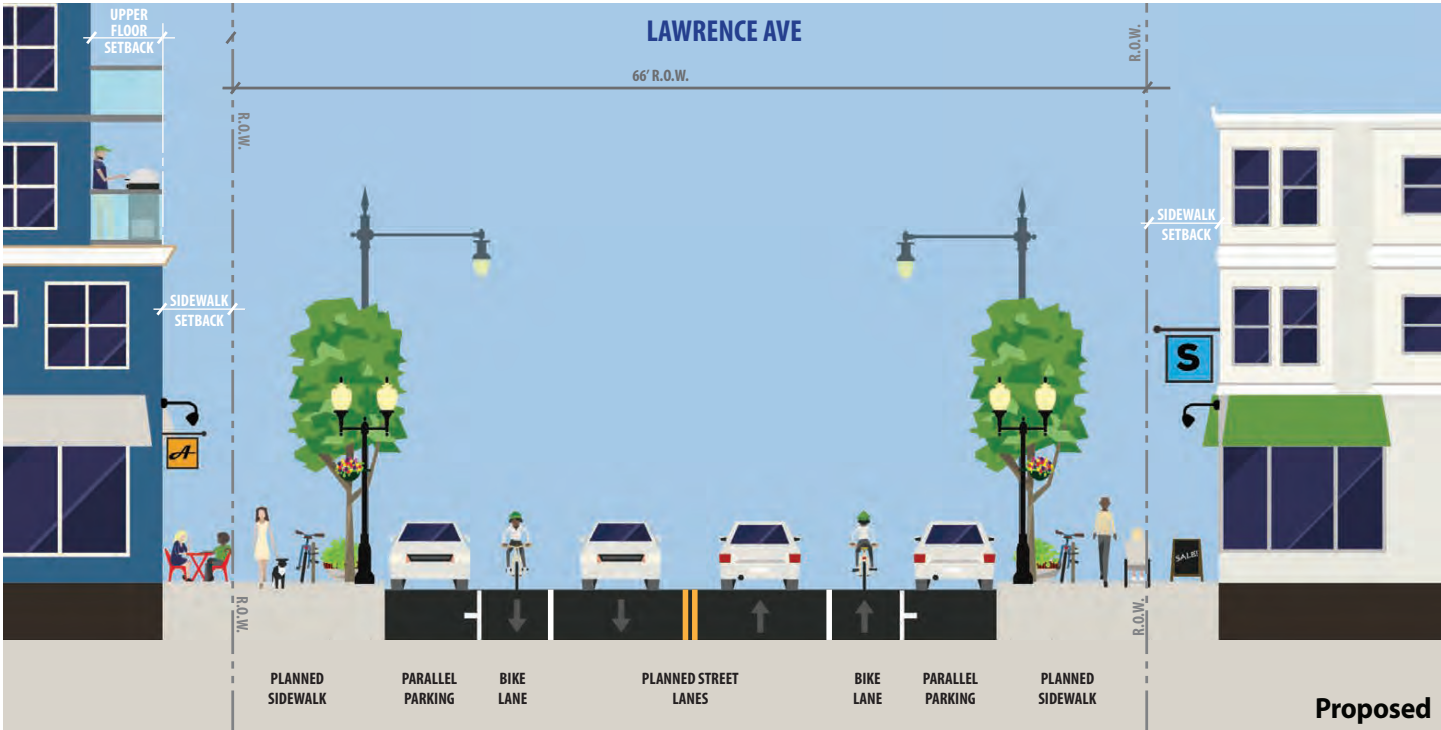
Views should remain clear and open. Plants along sidewalks should not exceed 3'-0" in height.

□ AMENITIES + SIDEWALK ACTIVATION

Spaces for outdoor activities should complement adjacent buildings. Features such as interactive art, seating and games are encouraged to activate the sidewalk areas.



Existing



Proposed



WESTERN AVENUE

AN AUTO ORIENTED CORRIDOR WITH ACCESS TO CTA, COMMUNITY INSTITUTIONS AND HIGH QUALITY NEIGHBORHOODS

Western Avenue includes a diverse mix of residences, shops and dining. However, high levels of auto traffic and a generally bland streetscape create an environment that repels pedestrians. The existing environment includes a mixture of primarily 2-4 story buildings oriented against the sidewalk or behind parking areas. Given this mixture, buildings in this area share an inconsistent edge and undefined street wall. Although sidewalks are relatively wide, they lack landscaped parkways and streetscape amenities. Future developments on Western Avenue should be more consistently aligned with the sidewalk and invite greater pedestrian activity.

What we heard

70% of respondents wished to mitigate the barrier feel of Western Avenue.

20% of respondents called for higher density, particularly near transit. Numerous comments specifically addressed the need for more affordable housing.

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Curb cuts should be kept to a minimum, and parking should be moved to the rears of buildings where necessary

0'-0" lot line setbacks should be encouraged to reinforce the street wall along Western Avenue

Modulation of upper floors should be encouraged to minimize monotonous exterior walls

Ground floor commercial uses should be encouraged to activate the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

New construction should be dense and include a variety of housing types

□ STREETScape + SIDEWALK ORGANIZATION

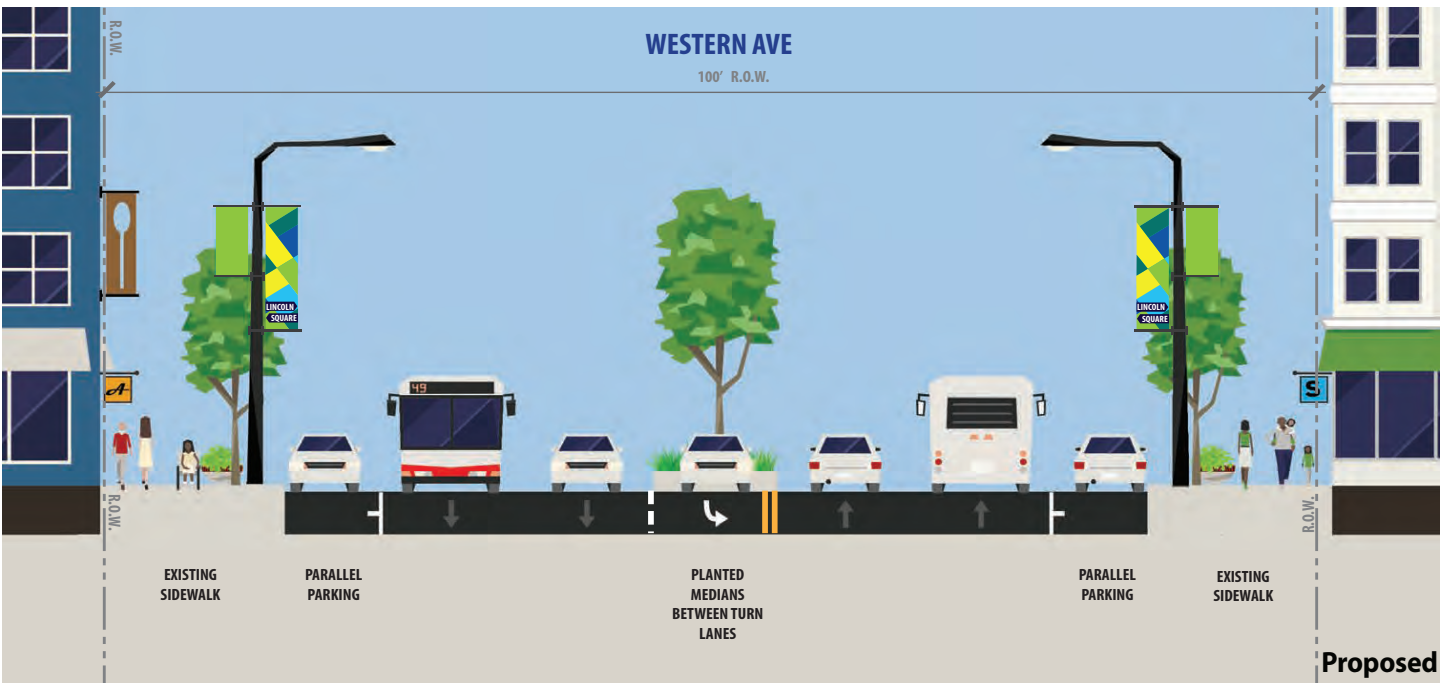
Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

Views should remain clear and open. Plants along sidewalks should not exceed 3'-0" in height.

□ AMENITIES + SIDEWALK ACTIVATION

Spaces for outdoor activities should complement adjacent buildings. Features such as interactive art, seating and games are encouraged to activate the sidewalk areas.



MONTROSE AVE

AN AUTO ORIENTED CORRIDOR WITH ACCESS TO CTA, COMMUNITY INSTITUTIONS AND HIGH QUALITY NEIGHBORHOODS

A small section of Montrose Avenue, west of Western Avenue, sits within the SSA boundary. This area includes 2-4 story buildings with a variety of forms and uses. Buildings are inconsistent in their placement; some are located against the sidewalk and some are located behind or adjacent to surface parking lots. The sidewalk varies in width, and surface treatments are also inconsistent. Because of these factors, outdoor gathering spaces are limited.

What we heard

Respondents expressed a need to improve the walkability and character of this area, including better connections between Montrose Avenue and other destinations.

Wayfinding signage and streetscape improvements were requested by numerous respondents.

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Minimize curb cuts and encourage parking at the rears of buildings

5'-0" lot line setbacks should be encouraged to maximize sidewalk and outdoor seating spaces

5'-0" setbacks should be encouraged for all floors above the second floor to reduce the "canyonization" effect

Ground floor commercial uses should be encouraged to activate the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

□ STREETScape + SIDEWALK ORGANIZATION

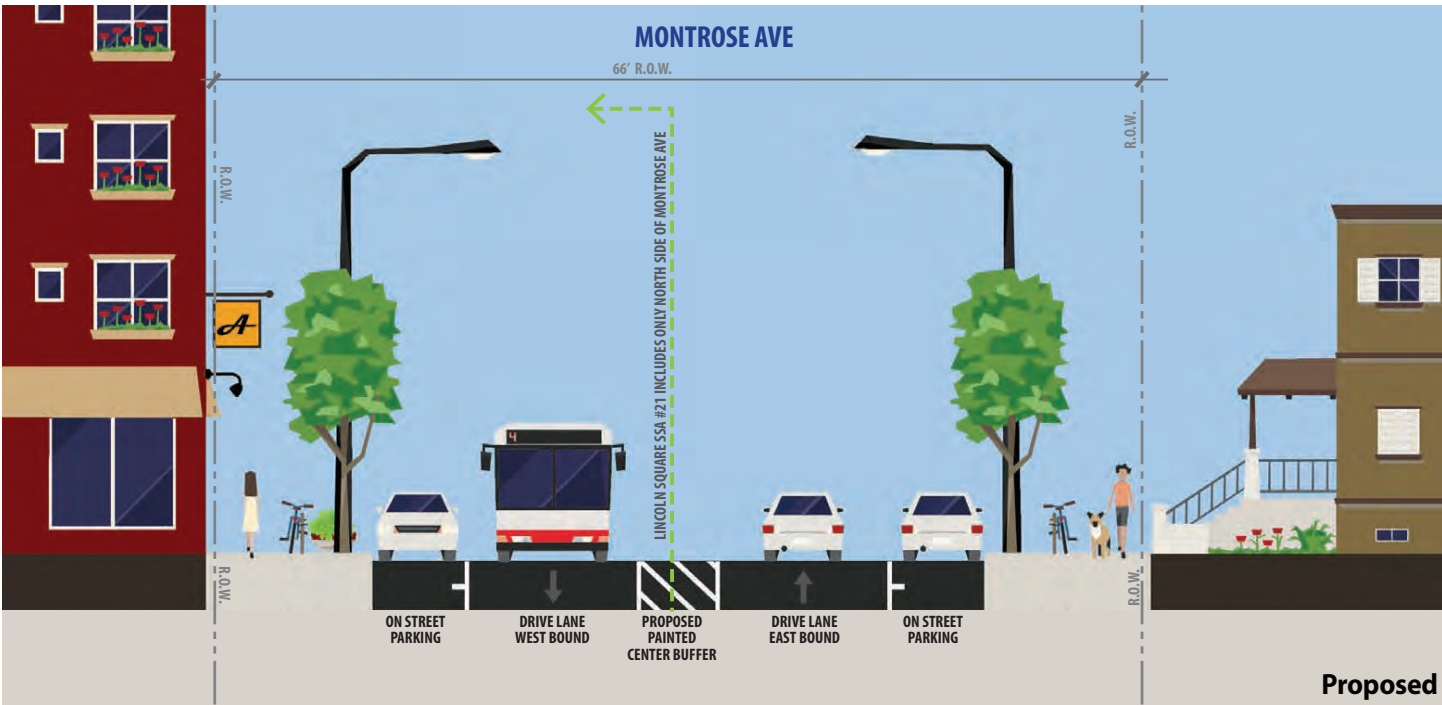
Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

Views should remain clear and open. Plants along sidewalks should not exceed 3'-0" in height.

□ AMENITIES + SIDEWALK ACTIVATION

Spaces for outdoor activities should complement adjacent buildings. Features such as interactive art, seating and games are encouraged to activate the sidewalk areas.



ROCKWELL CROSSING

A WALKABLE COMMERCIAL ENCLAVE WITH ACCESS TO THE CTA

Rockwell Street between Eastwood and Leland Avenues, or “Rockwell Crossing,” is a quaint neighborhood commercial district centered upon the CTA Brown Line. It is highly walkable and densely packed with small businesses. The relatively low (1-2 story) buildings are lined by narrow, yet well utilized sidewalks. As Rockwell Crossing is a transit hub with potential for future development, design guidelines should be used to protect the district’s walkability and community feel.

What we heard

20% of respondents called for higher density, particularly near transit. Multiple comments noted a desire for affordable housing.

Many comments reflected the need to maintain the quaint character of Rockwell Crossing, maintaining on street parking and support for incorporating wayfinding signs.

Respondents wished that Rockwell Crossing would host festivals and other events for the nearby community.

PRIVATE DEVELOPMENT GUIDELINES

□ BUILDING ORIENTATION + SITE LAYOUT

Characteristics of the existing street should be protected and maintained, including building scale and orientation to the sidewalk

Existing building facades should be maintained, including storefront windows, expressed cornice lines, building signage, landscaping and seating

Ground floor commercial uses should be encouraged to activate to the street

New facades should be largely transparent, including appropriately placed building signage, seating, landscaping and lighting

□ STREETScape + SIDEWALK ORGANIZATION

Outdoor seating and dining are positive elements that should be encouraged

Movable streetscape elements, such as planter pots, railings and seating, should be organized to maintain a clear and open sidewalk

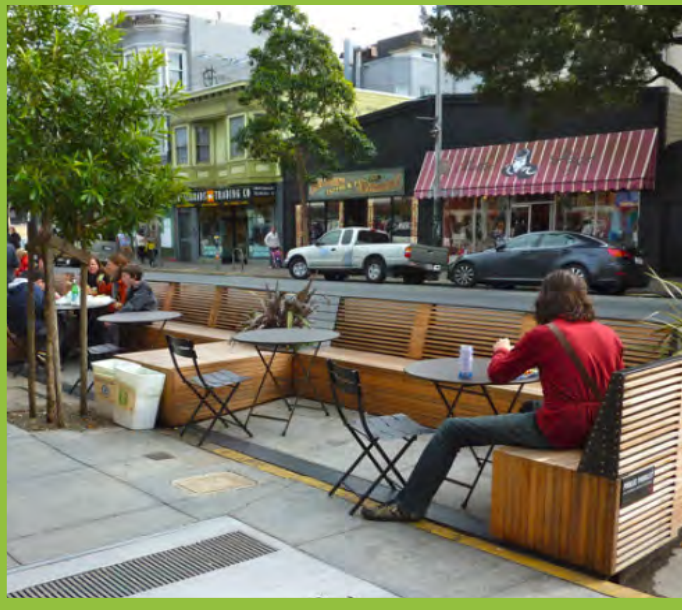
Views should remain clear and open. Plants along sidewalks should not exceed 3'-0" in height.

□ AMENITIES + SIDEWALK ACTIVATION

Spaces for outdoor activities should complement adjacent buildings. Features such as interactive art, seating and games are encouraged to activate the sidewalk areas.

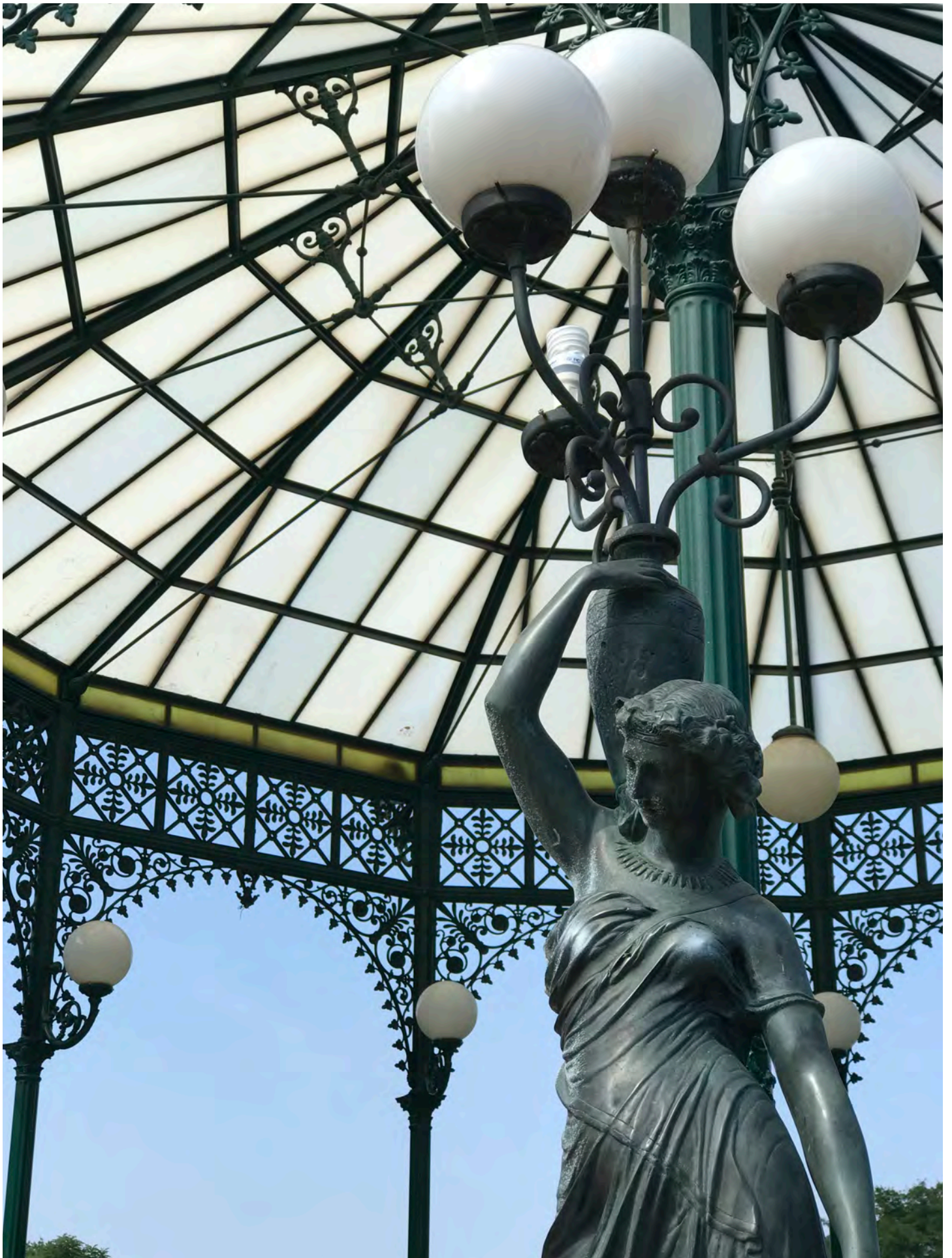


Parklet



Shared Street







CHAPTER 4 | REINFORCE PLACE

FOSTER PLACEMAKING + A VIBRANT COMMUNITY



CHAPTER 4 REINFORCE PLACE

Objective 1:

Identify and develop near term projects

Objective 2:

Partner with local agencies to develop long range projects



FOSTER PLACEMAKING + A VIBRANT COMMUNITY

Lincoln Square is a well connected community with access to the Western and Rockwell Brown Line CTA Stations, bus routes, Divvy stations, bike routes (both existing and planned), and connected sidewalks. Yet the safety and connectivity of pedestrian and bicycle transit, in particular, must be improved to connect residents to destinations.

Public places are vital to the health and vibrancy of Lincoln Square. These areas provide opportunities for special gatherings, celebrations, rallies, and day-to-day connections. As new buildings rise and the population grows, the need for common spaces only increases. Taking cues from established spaces like Lincoln Avenue, Giddings Plaza, and Welles Park, Lincoln Square should install more streetscapes, plazas, and parks in the years to come. Improvements may include near term installations and partnerships with local agencies for long range projects.

Installations include projects that have a big impact on the environment with low infrastructural cost. The SSA organizes, manages and often funds many of these projects including mural installations, hanging baskets, vinyl banners, bike racks and other arts projects like the “Parade of Abes” conversion of outmoded parking meters.

Partnerships with local agencies are required for larger scale infrastructural projects, such as the SSA’s agreement with CDOT to maintain recently installed rain garden planters located along east Lawrence Avenue and its consultations regarding the Lawrence Avenue streetscape expansion west of Western Avenue.





Objective 1: Identify and Develop Near Term Projects:

PROJECT CHECKLIST

- 1.1: Murals
- 1.2: Bike Racks
- 1.3: Mosaic Planters
- 1.4: Blade Signs
- 1.5: Parklets
- 1.6: Gateway Poles and Banners
- 1.7: Wayfinding and Business Kiosks
- 1.8: (SQU)Area Plazas

PROJECT 1.1

Murals

The SSA manages a successful program called 'PaintWorks' to fund public murals throughout the community. The SSA accepts rolling applications from property owners willing to host murals, with two hosts selected for murals every year. Requests for proposals are sent to artists, after which the SSA's Arts Committee selects a mural artist with input from the property owner. The program has had a tremendous positive impact on the community, especially in narrow corridors such as North Lincoln Avenue, where there is little space for other street improvements.

Actions

- Continue to work with property owners and local artists to install murals in key corridors



PROJECT 1.2

Bike Racks

The SSA is responsible for the design, installation and maintenance of branded bike racks and corrals. The SSA promotes cycling to encourage healthy living and sustainability. The SSA and LSRCC support CDOT in its efforts to convert Leland Avenue between the Chicago River and Clark Street into a low stress bike route or 'greenway'. This project will provide an important east-west connection for bicyclists who want to traverse the community without riding on busy streets.

Actions

- Continue to install bike racks near businesses and streets frequently used by cyclists



PROJECT 1.3

Mosaic Planters

Over the years, the SSA has purchased concrete planters of varying shapes and distributed them throughout the district. Although even the oldest of these planters are structurally sound, some lack visual interest. Feedback suggests an appetite for covering these planters with mosaic tiles, a relatively near term project that could dramatically enhance Western Avenue, in particular. Using tiles small enough to maintain the curvature of each planter, mosaics could address the history of Lincoln Square and guide visitors throughout the community.

Actions

- Work with a local artist to test and understand the materials, costs, and overall feasibility of covering planters with mosaic tiles
- Develop a program in which local artists would be assigned planters to cover with mosaic tiles



PROJECT 1.4

Blade Signs

Blade signs are mounted perpendicularly on the faces of buildings, oriented towards pedestrians on the sidewalk. These signs are relatively low cost to fabricate, and yet they can make tremendous visual impacts on a business corridor. In some communities, business owners, signage fabricators and municipal representatives work together to design, fabricate and install blade signs in large groupings, thereby minimizing cost and procedural delays. Local signage codes would apply to blade signs, and permits would likely need to be secured.

Actions

- Review local sign codes and ordinances
- Identify a cluster of businesses to test a blade signage program
- Secure a local sign fabricator
- Secure permits with City of Chicago to place signs in desired areas
- Host a workshop to design signage that evokes specific businesses and meets local codes
- Work with the sign fabricator to install signs



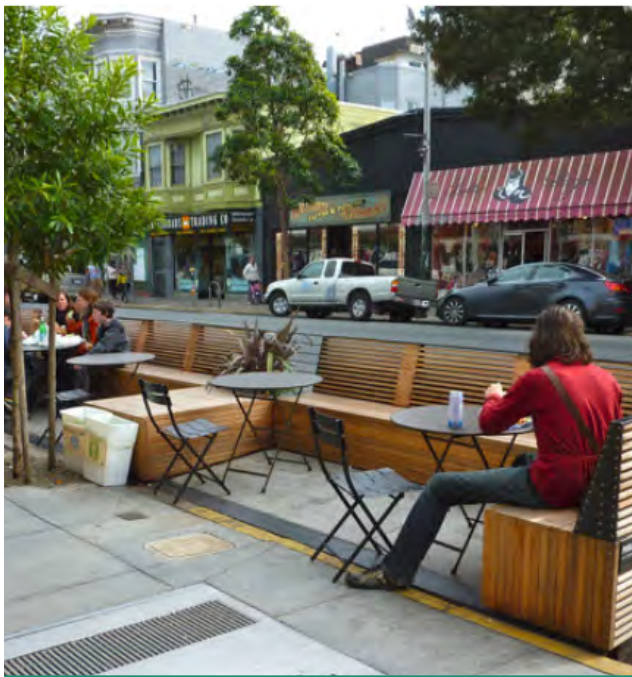
PROJECT 1.5

Parklets

Where sidewalks are too narrow to support outdoor seating and dining, parklets may be used instead. Parklets are temporary seating areas located in place of a parallel, on street parking space. Parklet perimeters are typically defined by bollards, planter pots, railings or painted walls, all assembled on weather proof decking. Parklets are most successful when located adjacent to land uses that provide constant activity, like cafes and restaurants, and near businesses that can assist with maintenance.

Actions

- Obtain information from CDOT regarding parklet design standards
- Identify a business to sponsor a test parklet
- Design a parklet with input from the business and community
- Secure permits with CDOT to place parklets in desired areas
- Install the parklet and activate it with regularly scheduled events



PROJECT 1.6

Gateway Poles + Banners

The streetscaped portion of Lawrence Avenue, east of Western Avenue, is lined with decorative banners and “gateway poles” that feature artistic metalwork. More of these poles could be erected throughout Lincoln Square, preferably the entry points to key corridors and spaces. Suggested locations for gateway poles include proposed median landscapes along Western Avenue, the Lincoln Statue Plaza and Rose Garden (Ainslie Plaza) area east of McDonald’s.

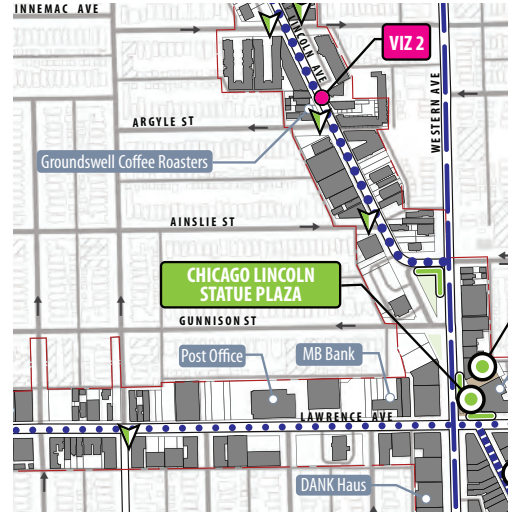
Actions

- Design gateway poles based on existing versions along east Lawrence Avenue
- Secure permits with CDOT to place new poles in desired areas
- Install gateway poles and banners in areas needing better branding, as identified by the community





EXISTING



Quick Poll #1: What about Lincoln Square needs the most improvement?

- 'Traffic Congestion' (309 votes, 55%)
- 'Pedestrian & Bike Amenities' (219 votes, 39%)



View looking along Lincoln Avenue south towards Argyle Street: This photo visualization addresses improvements that enhance the pedestrian, transit, bike and retail connections along Lincoln Ave, north of Lawrence.

PROJECT 1.7

Wayfinding & Business Kiosks

Signage kiosks provide opportunities to promote local businesses and encourage exploration of Lincoln Square. Kiosks could be double sided cabinets, periodically updated with maps of nearby businesses and other public information. Alternatively, kiosks could be digital, providing changeable messages.

Actions

- Design a business sign kiosk prototype
- Secure permits with CDOT to place the prototype kiosk in a visible area
- Erect business kiosks in areas needing better signage, as identified by the community



“Lincoln SQU(AREA) = Impactful & Awesome: The artwork/mini plaza on North Lincoln helps visually connect North Lincoln to the Square, while being a great neighborhood gathering spot. Our neighbors along North Lincoln would love this!”

-Comment posted to project website.

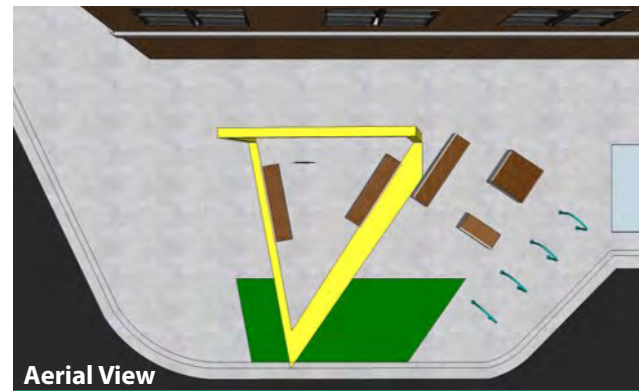
PROJECT 1.8

(SQU)Area Plazas

At select intersections in Lincoln Square, particularly in the northern Lincoln Avenue corridor, sidewalks could be widened to house mini (SQU)Area plazas. These plazas might, for instance, include some form of sculpture that uses forced perspective to artistically interpret the square shape. The sculptures could adopt different forms and be paired with mural art or landscape plantings. The (SQU)Areas could include clustered seating to provide additional places for community members to gather.

Actions

- Select locations to construct (SQU)Areas that have active, adjacent land uses
- Work with CDOT to design and permit (SQU)Areas
- Install (SQU)Areas and activate them with regularly scheduled events



When asked 'Which site-specific improvements would you like to see most?', 45% of respondents listed 'shared street on Lincoln Avenue'



Objective 2: Partner with Local Agencies To Develop Long Range Projects

In addition to initiatives targeted to specific locations, some larger scale, longer term projects would have ripple effects across the entire neighborhood. Long term projects would likely require coordination and permitting with regulatory agencies, such as CDOT and CTA. As such, concepts presented in this Plan are provided for planning and discussion purposes only, and depend on other inputs to be provided by partners.

PROJECT CHECKLIST

- 2.1: Lincoln Avenue Festival and Market Street
- 2.2: Giddings Plaza / Alleyway
- 2.3: CTA Station Plaza / Pedestrian Link beneath the CTA tracks / Market and Events Plaza
- 2.4: Lincoln Statue Plaza
- 2.5: Neighborhood Flex Space
- 2.6: Lawrence Avenue Complete Street
- 2.7: North Lincoln Avenue Complete Street
- 2.8: Western Avenue Boulevard-ization
- 2.9: Rockwell Crossing Shared Street

Urban Design Framework



PLACEMAKING + STREETScape

RECENT & PLANNED IMPROVEMENTS:

●●●●● CDOT Lawrence Ave Streetscape Program

MASTER PLAN PROPOSALS:
 — BOULEVARDIZATION
 — FESTIVAL STREET
 — COMPLETE STREET

○ KEY NEIGHBORHOOD PLACEMAKING OPPORTUNITIES

▽ SIDE STREET 'SQU(ARE)' SPACE OPPORTUNITIES

— GATEWAY/NEIGHBORHOOD IDENTITY FEATURE

● VIZ = PHOTO VISUALIZATION LOCATION

MAP LEGEND:

— SSA #21

— Parks and Green Space

— Plaza

M Brown Line Station

Gateway Poles & Banners



Market Street



Side Street Bulb



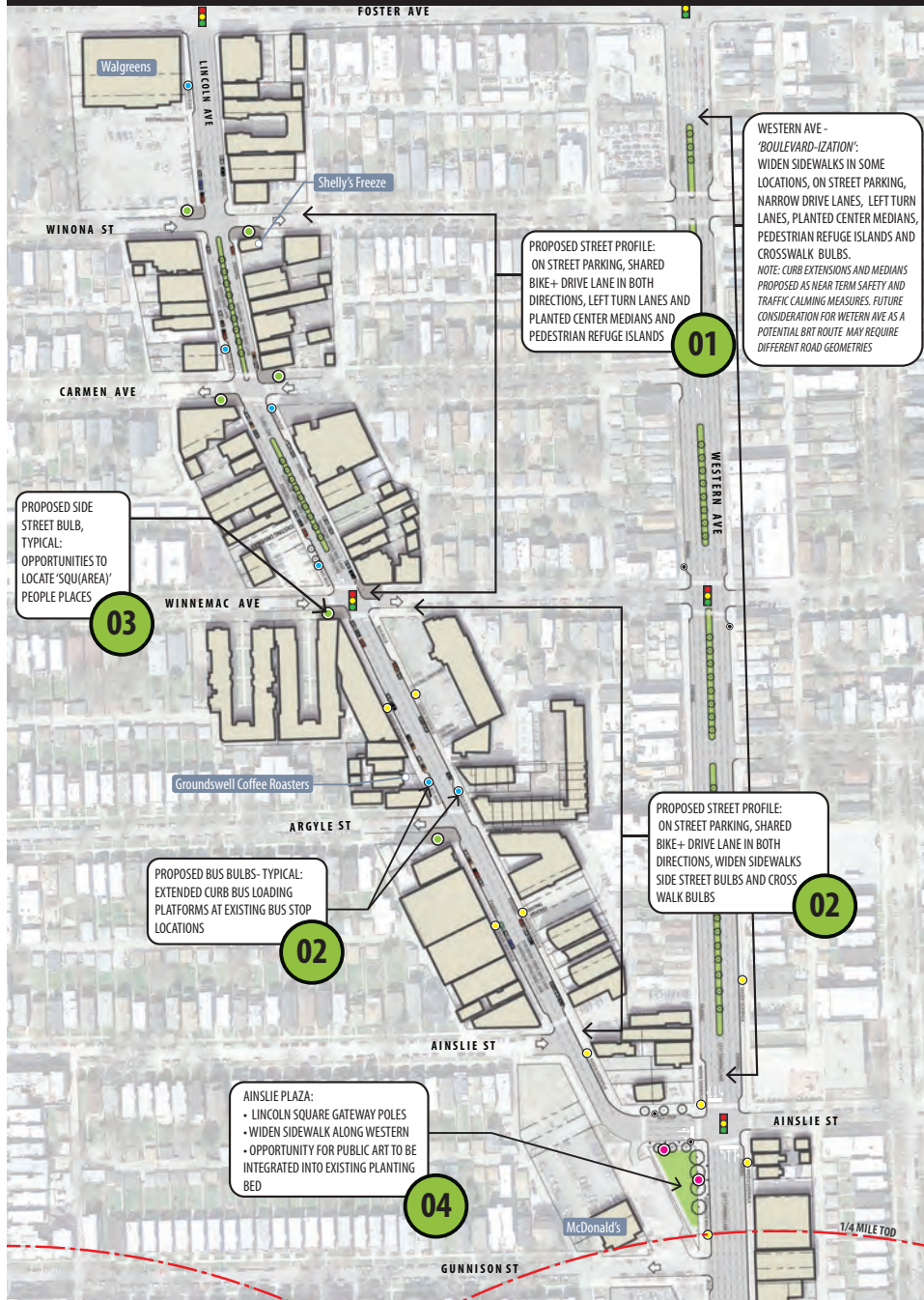
Neighborhood Flex Space



Lincoln Avenue | North Lincoln

Quick Poll #1: What about Lincoln Square needs the most improvement?

- 'Traffic Congestion' (309 votes, 55%)
- 'Pedestrian & Bike Amenities (219 votes, 39%)



- URBAN DESIGN STUDY LEGEND:**
- GATEWAY / IDENTITY FEATURE
 - 'SQU(AREA)' PEOPLE SPACE OPPORTUNITY
 - BULB BULB- BUS LOADING SIDEWALK PLATF
 - WIDEN SIDEWALK
 - BUS STOP
 - EXISTING CURB TO REMAIN
 - PROPOSED CURB LINE
 - EXISTING CURB LINE, TO REMOVE

(01) Median / Refuge Islands



(02) Bus Bulbs



(03) Side Street Bulb



(04) Gateway Poles & Banners



Western Ave-Lincoln Ave | Central

Quick Poll #2: Which site specific improvements would you like to see the most?

- 'Mitigating the barrier feel of Western Avenue' (485 votes, 70%)
- 'Enhance Western CTA Station plaza / pedestrian underpass (455 votes, 65%)
- 'Enhance the plaza surrounding Lincoln's statue' (341 votes, 49%)



(01) Gateway Poles & Banners



(02) Station Plaza



(03) Planted Roadway Median



(04) Gateway Sign at Tracks



PROJECT 2.1

Lincoln Avenue Festival and Market Street

The Square is a well loved community space. Day to day, it provides a place for shopping, dining and places to gather. During special events, this space is converted to a lively festival and market street. While the street is beloved, some functional concerns must be addressed, including a need for traffic calming towards the north end of the street, improved sight lines, enhanced pedestrian access across the street and better accommodations for fests and events. Two alternative concepts have been tested for consideration:

Concept A evaluates converting diagonal parking along the east side of the street to parallel parking along the east and west sides of the street. This configuration increases parking by one additional space, incorporates additional mid-block crossing bump outs and improves sight lines to Giddings Plaza and across the street. Business deliveries could still be accommodated at select times of the day. This concept could also include a curbless street that would better accommodate events, as well as overhead light canopies that would contribute to a festive atmosphere.

Concept B evaluates a chicane environment that alternates diagonal parking spaces between the east and west sides of the road. This configuration reduces on-street parking by six spaces. The concept incorporates additional mid-block crossing bump outs, improved sight lines to Giddings Plaza, decorative pavements and an optional overhead light canopy.



Both concepts could be tested with simple, temporary paint markings. This would allow the SSA and community to determine pros and cons for both concepts before investing in costly construction.

Actions

- Review preliminary concepts with CDOT representatives to discuss logistics for each
- Work with a designer to develop project budgets and determine funding opportunities
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options and testing
- Work with CDOT to conduct a traffic test using paint and signage to simulate the selected parking and streetscape pattern
- Monitor the test, record data and maintain outreach with businesses and residents to record reactions
- Work with a designer and engineer to develop final design plans and budgets
- Request proposals for construction of the final concept

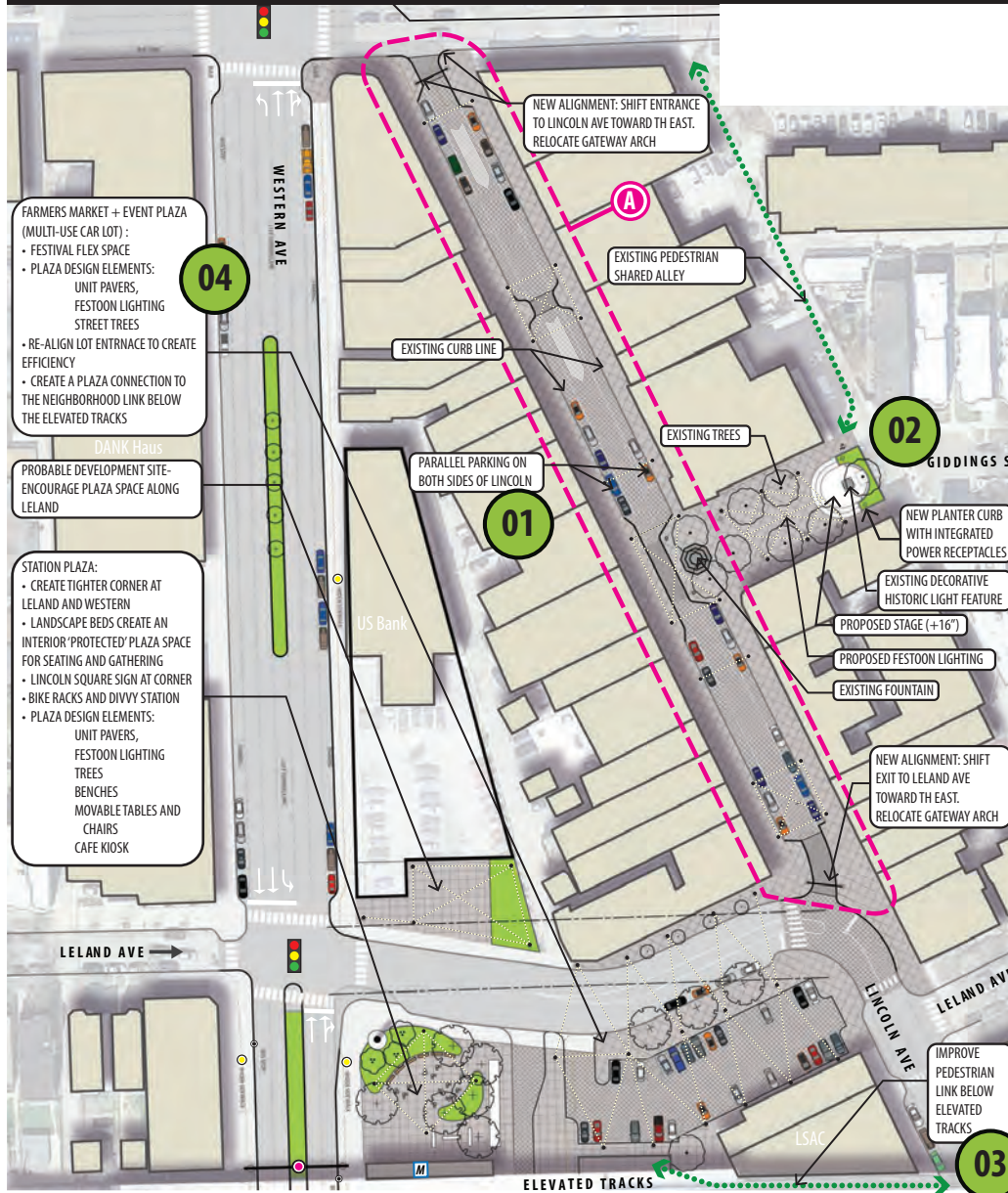
“Improve walkability, without closing streets: Traffic calming (better pedestrian crossings, slower cars, etc.) is critical to keeping Lincoln Square walkable. However, if we want thriving retail, streets must remain open to cars. It’s about how best do design streets to welcome cars, pedestrians and bicyclist.”

-Comment posted to the project website.

Lincoln Avenue | The Square: *Concept A*

Quick Poll #2: Which site-specific improvements would you like to see most?

- 'Shared street on Lincoln Ave' (313 votes, 45%)



(01) Concept A: Parallel Street Parking with Traffic Calming



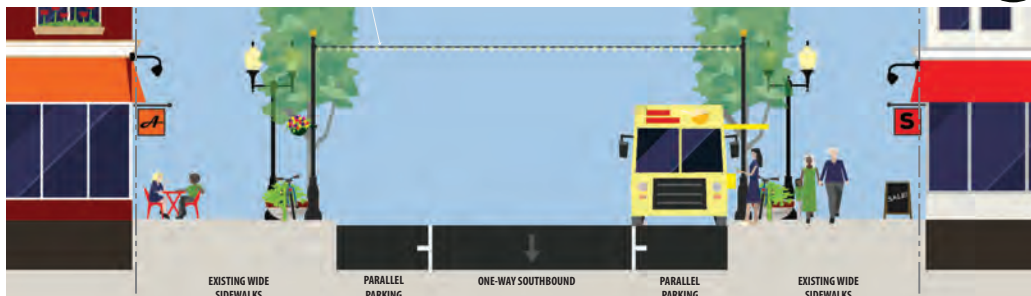
(2) Improved Paving, Lighting & Stage Relocation at Giddings Plaza



(03) Improved Pedestrian Link Below the Elevated Tracks



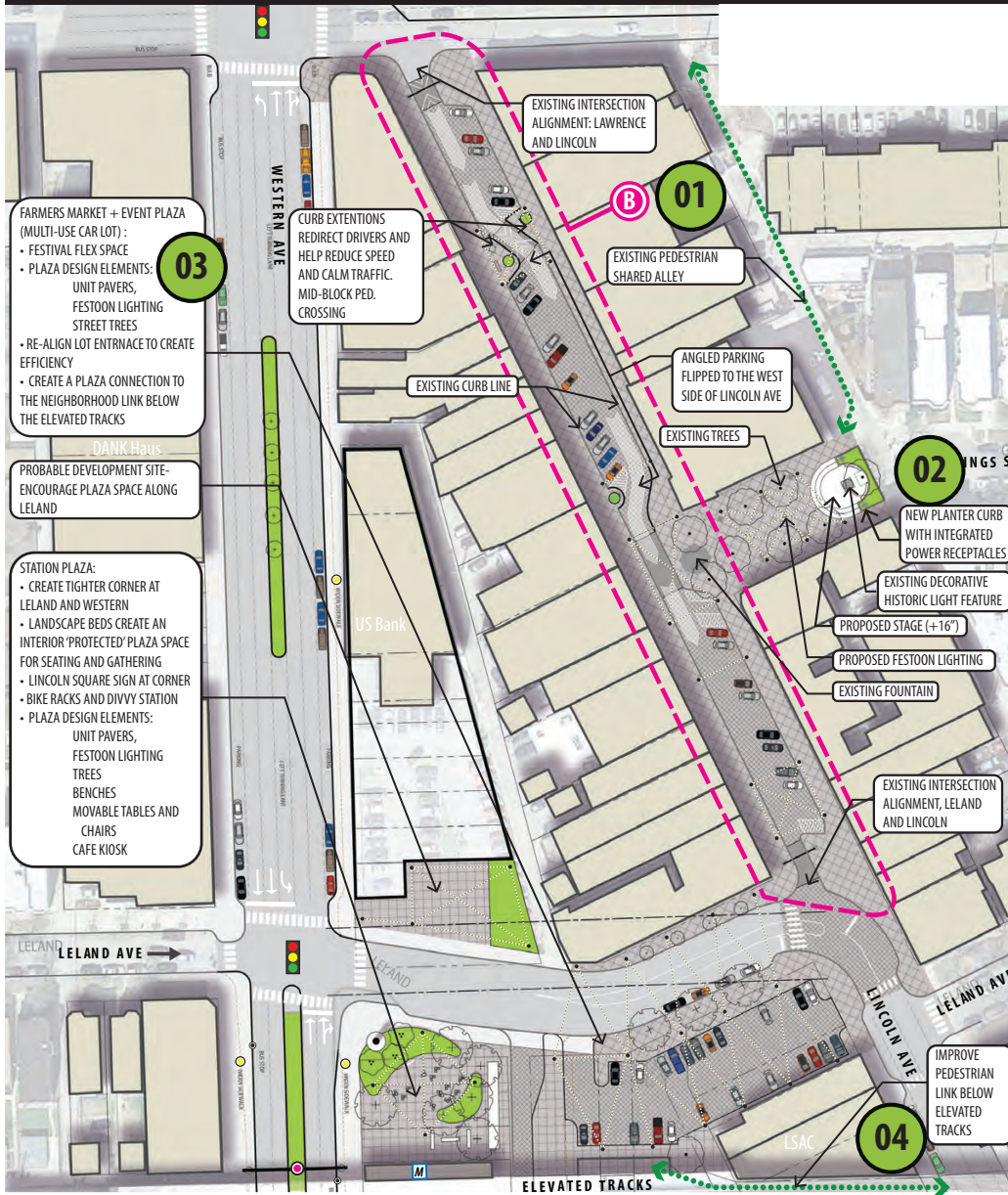
(04) Farmers Market + Event Plaza Improvements



Lincoln Avenue | The Square: *Concept B*

Quick Poll #2: What types of public art would you like to see in Lincoln Square?

- 'Street canopy' (348 votes, 25%)



(01) Concept B: Angled Street Parking with Traffic Calming



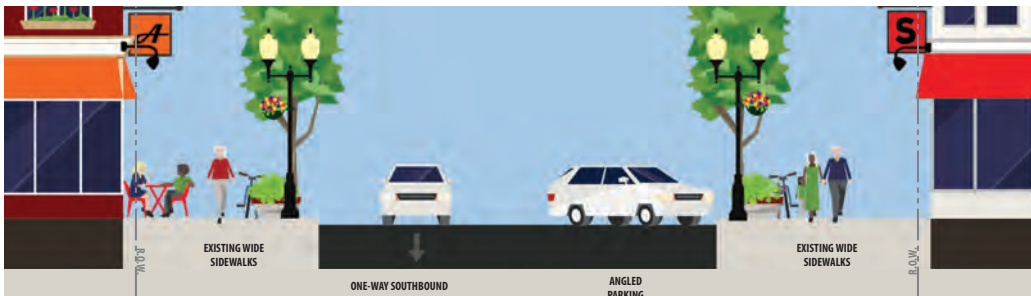
(2) Improved Paving, Lighting & Stage Relocation at Giddings Plaza



(03) Farmers Market + Event Plaza Improvements



(03) Farmers Market + Event Plaza Improvements



PROJECT 2.2

Giddings Plaza/Alleyway

Giddings Plaza has become synonymous with Lincoln Square. The Fountain, Lombard Light and quaint space between buildings is a huge draw for families, especially during special events. While the plaza is treasured, even it could use improvement. Its raised stage sits awkwardly in the middle of the plaza, and electricity for the stage is unreliable.

Rather than using this built in stage, the LSRCC spends a large portion of its limited resources to erect temporary stages at the rear of the plaza in order to accommodate performers and crowds. Additionally, the alleyway located to the east of the plaza has become a dangerous environment for kids darting into and out of the plaza. The concept removes the central raised stage to open the plaza and proposes a new stage surrounding the Lombard Light, featuring seating steps and a raised planter wall toward the rear of the stage, along the alleyway. This wall would act as a physical barrier, helping to control pedestrian activity around the plaza. An overhead light canopy is proposed to provide additional ambiance.

Actions

- Review preliminary concepts CDOT representatives to discuss logistics and permitting
- Work with a designer to develop project budgets and determine funding opportunities
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape improvement plans
- Work with a designer and engineer to develop final design plans and budgets
- Request proposals for construction of the final concept

When asked 'what types of public art would you like to see in Lincoln Square?', 25% of respondents identified a **'street canopy'**.



PROJECT 2.3

CTA Station Plaza/Pedestrian Link Beneath the CTA Tracks/ Market and Events Plaza

The CTA Western Avenue brown line station serves thousands of travelers each month and functions as a gateway to the community. Public spaces surrounding the station, including the plaza to the north, parking lot and bus lane to the east, and passageway under the tracks connecting the station to Lincoln Avenue, all need substantial improvement. Redeveloping these spaces would improve visual continuity and access to the area's many shops and restaurants.

Proposed CTA Station Plaza enhancements improve pedestrian access to and from the station area and Divvy bike racks, while also including spaces for gathering and relaxation. Landscaping include low plantings and canopy trees to provide buffering between the plaza and street. Seating areas are strategically placed to accommodate individual and group gathering. The new plaza might also feature a segment of the Berlin Wall, which currently resides inside the station, as a nod to the neighborhood's German heritage.

The pedestrian link beneath the CTA tracks is a convenient, if unsightly, shortcut from the station to Lincoln Square's shops, dining and neighborhoods to the east. This dark and uninviting space could be vastly improved with non-structural improvements such as lighting, paving, plantings, seating and public art.

The station's adjacent public parking lot is home to the neighborhood's summer farmers markets, as well as various festivals throughout the year. This space could continue to accommodate cars while benefiting from functional and aesthetic improvements such as access to power, lighting, decorative paving and landscaping.

Actions

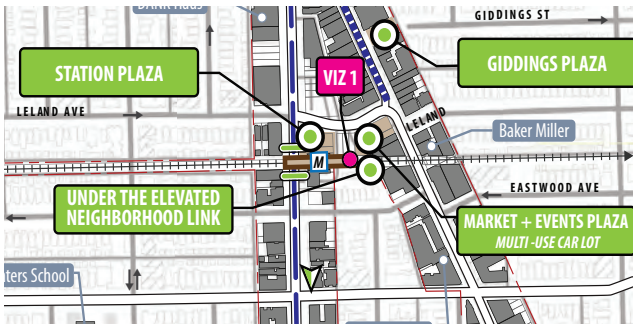
- ❑ Review preliminary concepts with CDOT and CTA representatives to discuss logistics and permitting
- ❑ Work with local artists and designers to install near term projects , such as mosaic planters and murals, to build near term support
- ❑ Work with a designer to develop project budgets and determine funding opportunities
- ❑ Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options
- ❑ Work with a designer and engineer to develop final design plans and budgets
- ❑ Request proposals for construction of the final concept

“Western Avenue (by the L stop): With all the foot traffic coming through the L stop, bus transfers, and Lincoln Ave, it would be nice to see more pedestrian-friendly shops and developments on Western between Montrose and Argyle, just north of Lawrence. The Western L transit stop is a hub for people coming from Lincolnwood, Lincoln Square, and practically anywhere in the city with the Western express bus. A revitalized station and surrounding area would be great!”

-Comment posted to project website.

“The plaza outside the L station is nearly unusable - there are a couple chairs and the Divvy bike dock, but that large space could be used much more effectively. Same thing with the little plaza outside Walgreens on Lawrence/ Lincoln.”

-Quote taken from project website



Quick Poll #2:

Which site-specific improvements would you like to see the most?

- ‘Enhance Western CTA Station plaza / pedestrian underpass (455 votes, 65%)



View looking from the CTA Brown Line station (beneath the tracks) east towards the Farmer’s Market / Events Plaza: This photo visualization addresses the connection between the Western Avenue Brown Line station and surrounding shops and neighborhoods. Lighting, art, signage, pavement, seating and plantings are placed independent from existing CTA structures to improve the quality of the pedestrian environment.



Lincoln Square Farmers Market



The Lakeview Low-Line



Apple Fest

PROJECT 2.4

Lincoln Statue Plaza

The Plaza at the northeast intersection of Lincoln and Lawrence Avenues, or the “Lincoln Statue Plaza,” for its iconic statue of the neighborhood’s namesake President, is in need of significant upgrades. As the center of the Lincoln Square SSA, this plaza serves as a major gateway point. Pedestrian friendly improvements surrounding the plaza would help bridge barriers created by high traffic streets, ensuring a seamless corridor between The Square and upper Lincoln Avenue.

Actions

- ❑ Review preliminary concepts with CDOT and CTA representatives to develop a traffic analysis and study perimeter plaza improvements, including improved crossings, bump outs and gateway signage
- ❑ Work with a designer and engineer to develop project plans and budgets for interior plaza improvements, and to determine funding opportunities
- ❑ Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss plaza programming and options
- ❑ Work with a designer and engineer to develop final design plans and budgets
- ❑ Prepare project for implementation via RFP or local contracts as budgets dictate

When asked ‘which site-specific improvements would you like to see the most?’, 49% of respondents called for **‘enhancing the plaza surrounding Lincoln’s statue’**.

PROJECT 2.5

Neighborhood Flex Space

The current community garden space located north of the Western Avenue Walgreens building could potentially become a place for neighborhood flex uses. The space may be reconfigured to accommodate multiple functions requested by the community, such as a dog park. The space could also host pop up shops and food trucks, or be enhanced to continue serving community gardening needs.

Actions

- ❑ Review preliminary concepts with Aldermanic offices to discuss project logistics
- ❑ Work with a designer to develop project budgets and determine funding opportunities
- ❑ Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss flex space programs and enhancements
- ❑ Work with a designer and engineer to develop final design plans and budgets
- ❑ Request proposals for construction of the final concept

“I would love a dog park, even better if it had a small dog area like the Logan Square dog park does!”

-Comment posted to project website.

PROJECT 2.6

Lawrence Avenue Complete Street

Lawrence Avenue east of Western was recently reconstructed to accommodate bike lanes, widened sidewalks and rain gardens. The SSA has recently assumed control over the rain garden areas. In the near term, CDOT plans to extend the streetscape along Lawrence Avenue west of Western Avenue, where the profile would match that of the east section. The SSA should work with CDOT to ensure that site furnishings are appropriately located along the street. In addition, mini plazas, or “(SQU)Areas,” should be considered in key side street areas as appropriate.



Actions

- Continue working with CDOT to guide streetscape construction west of Western Avenue
- Communicate streetscape plans and relevant building design guidelines with prospective developers, establishing a common vision for the corridor’s long term revitalization
- Work with Aldermanic offices to determine opportunities for near term enhancements
- Work with a designer to develop project budgets and determine funding opportunities for near term enhancements
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape enhancements
- Work with a designer and engineer to develop final design plans and budgets
- Request proposals for construction of the final concept

When asked ‘What about Lincoln Square needs the most improvement?’, 55% of respondents reported **‘traffic congestion’** and 39% of respondents reported **‘pedestrian and bike amenities’**.

PROJECT 2.7

North Lincoln Avenue Complete Street

Converting north Lincoln Avenue into a “complete street” would serve to support multiple modes of travel along the roadway. Bus bulbs, for example could extend sidewalks to meet buses where they stop for passengers, shortening boarding times and reducing cross traffic between buses and bicyclists. Bicycle lanes could be demarcated with roadway “sharrows” and other signage. South of Winnemac Avenue, opportunities exist to narrow the street and widen sidewalks, while the center turn lane north of Winnemac Avenue could, in certain places, be converted into a landscaped median.

Actions

- Review preliminary concept with CDOT and CTA representatives to develop a traffic analysis and study for streetscape improvements, including bus bulbs, medians and widened sidewalks
- Upon review of the traffic analysis, determine roadway and streetscape requirements
- Work with a designer and engineer to develop project plans and budgets, and to determine funding opportunities
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options
- Work with a designer and engineer to develop final design plans and budgets
- Request proposals for construction of the final concept

PROJECT 2.8

Western Avenue Boulevard-ization

The City of Chicago’s Vision Zero program has led to improvements along Western Avenue, such as raised pedestrian refuge crossings at key intersections. Building upon these improvements, Western Avenue could incorporate a landscaped boulevard along the roadway while still retaining left turn lane access at intersections. Gateway signage aligned with the CTA structure is proposed along Western Avenue, north and south of the CTA tracks.

Actions

- Review preliminary concepts with CDOT and CTA representatives to develop a traffic analysis and study for streetscape improvements, including landscaped medians, improved crossings, bump outs and gateway signage
- Upon review of the traffic analysis, determine roadway and streetscape requirements
- Work with a designer and engineer to develop project plans and budgets, and to determine funding opportunities
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options
- Work with a designer and engineer to develop final design plans and budgets
- Request proposals for construction of the final concept

When asked ‘which site-specific improvements would you like to see the most?’, *mitigating the barrier feel of Western Avenue* received 70% of respondent votes.



When asked 'what do you like most about Lincoln Square?', 79% of respondents reported '**walkable environment**' and 71% of respondents reported '**quality of local shops, restaurants and businesses**'.

PROJECT 2.9

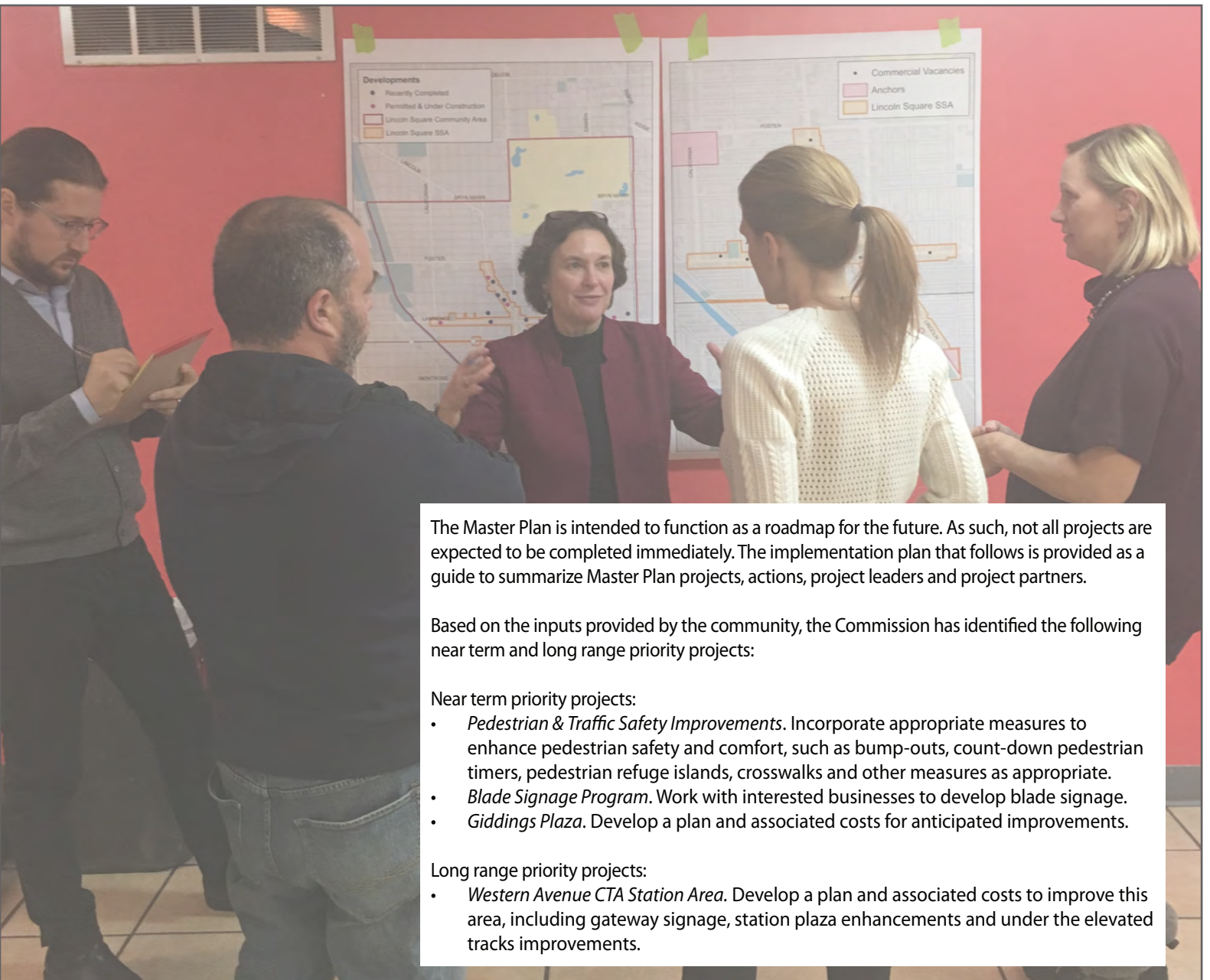
Rockwell Crossing Shared Street

Improvements in the area of Rockwell Crossing should enhance its walkable character and promote local shops and businesses. During outreach, the public expressed a desire to bring community festivals, events, and greater activity to this area. Proposed improvements therefore include temporary parklets in the near term and shared street improvements in the long term. These could include curbless streets and/or decorative pavement treatments that support festivals and events. Additional enhancements could include specialty lighting, landscaping and site furnishings.

Actions

- Work with a designer and engineer to develop preliminary concept plans for Rockwell Crossing
- Work with local artists and designers to install near term projects, such as parklets and seating areas, to build local support
- Review preliminary concepts with CDOT and CTA representatives to discuss logistics and permitting for streetscape improvements
- Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options
- Work with a designer and engineer to develop project budgets and determine funding opportunities
- Request proposals for construction of the final concept





The Master Plan is intended to function as a roadmap for the future. As such, not all projects are expected to be completed immediately. The implementation plan that follows is provided as a guide to summarize Master Plan projects, actions, project leaders and project partners.

Based on the inputs provided by the community, the Commission has identified the following near term and long range priority projects:

Near term priority projects:

- *Pedestrian & Traffic Safety Improvements.* Incorporate appropriate measures to enhance pedestrian safety and comfort, such as bump-outs, count-down pedestrian timers, pedestrian refuge islands, crosswalks and other measures as appropriate.
- *Blade Signage Program.* Work with interested businesses to develop blade signage.
- *Giddings Plaza.* Develop a plan and associated costs for anticipated improvements.

Long range priority projects:

- *Western Avenue CTA Station Area.* Develop a plan and associated costs to improve this area, including gateway signage, station plaza enhancements and under the elevated tracks improvements.

CHAPTER 5 | IMPLEMENT

DEVELOP PROJECTS STRATEGICALLY

TRANSPORT | IMPROVE CONNECTIONS FOR ALL MODES: IMPLEMENTATION PLAN

PROJECT	ACTIONS	LEAD	PARTNERS
Objective 1. Improve safety for people walking and biking			
1.1 Build Upon Vision Zero	<ul style="list-style-type: none"> Improve bike and pedestrian crossings at key connection points and where crashes are concentrated. Establish a network of low stress bicycle facilities throughout the neighborhood, building off of recent and planned facilities along Lincoln Avenue and Leland Avenue. 	CDOT	SSA
1.2 Improve Access to Transit	<ul style="list-style-type: none"> Prioritize pedestrian connections and crossing enhancements at bus stops with high ridership. Partner with CTA to improve aesthetics and access to the station areas. Partner with CDOT and CTA to develop gateway signage in front of the CTA structure at Western Avenue. Partner with CDOT and CTA to add wayfinding signage to direct travelers from station areas to Lincoln Square businesses. Partner with CDOT and CTA to improve the plaza around the Western Brown Line station and beneath the Elevated structure, adding enhanced lighting, pavements, plantings and furnishings. 	SSA	CTA/CDOT
Objective 2. Improve Traffic Flow			
2.1: Evaluate East Lawrence Improvements to Inform the West Lawrence Streetscape	<ul style="list-style-type: none"> Work with Aldermanic offices to guide site developments that are reflecting of the pedestrian oriented nature of Lawrence Avenue. Partner with CDOT to ensure bench placement is compatible with adjacent land uses and suited to pedestrian comfort. Insofar as the SSA is responsible for landscape maintenance, partner with CDOT to appropriately select and install new plantings. 	SSA/Aldermanic offices	CDOT
2.2: Identify Potential Improvements to Traffic Signal Timings	<ul style="list-style-type: none"> Call for volunteers to take an inventory of all pedestrian signals within the SSA. Identify with CDOT opportunities to upgrade pedestrian signals in conjunction with other projects. Work with Aldermen to study signal timings and identify places where signals could be added, upgraded, or adjusted. 	SSA / Volunteers	CDOT
Objective 3. Reinforce a sense of place			
3.1: Maintain Special Event Space That Complements and Connects the Western Avenue Brown Line Station to Businesses on Lincoln Avenue	<ul style="list-style-type: none"> Continue to program community events at the parking lot abutting the Western Brown Line station. Develop a concept plan in coordination with CDOT and CTA to enhance the parking lot with future improvements, including access to power, enhanced surface treatments, and landscaping. 	SSA	CTA/CDOT

PROJECT	ACTIONS	LEAD	PARTNERS
3.2 Establish Gateway and Wayfinding Signage for All Modes of Travel	<ul style="list-style-type: none"> Partner with CDOT to develop a conceptual wayfinding sign plan. Partner with local businesses to develop a business sign kiosk that promotes their locations. Partner with CTA and CDOT to develop gateway signs at key locations throughout the SSA. 	SSA/Aldermanic offices	CTA/CDOT
3.3 Further Encourage Pedestrian Activity by Creating a Flexible Streetscape Along Lincoln Avenue	<ul style="list-style-type: none"> Conduct a streetscape test using temporary elements such as paint and barricades. Evaluate alternative roadway and parking strategies such as chicanes, bump outs, and parallel parking arrangements. Develop a survey to understand outcomes of the test. In partnership with CDOT, develop a concept plan based on the test and survey outcome. 	SSA/Aldermanic offices/local businesses	CDOT

Objective 4. Improve Connections

4.1 Identify Site Specific Solutions to Barriers That Disconnect South and North Lincoln Avenue	<ul style="list-style-type: none"> Enhance Lincoln Ave with gateway enhancements at the Lincoln Statue Plaza and the triangular green space adjacent to McDonalds. Partner with CDOT to improve signal timing at key intersections with Lincoln Avenue. Partner with CDOT to improve Lincoln Avenue's pedestrian crossings with striping and countdown timers. 	SSA	CDOT
4.2 Identify and Promote Low Stress North-South and East-West Bike Routes	<ul style="list-style-type: none"> Conduct neighborhood bike rides and follow up surveys regarding proposed low stress bike routes to understand demand and build support. Work with local businesses and bicycling advocates to promote low stress bike routes. Partner with CDOT to develop plans for low stress bike routes, marked with formal signage and equipped with appropriate traffic calming devices. 	CDOT	SSA/Local bike groups
4.3 Prioritize Connections to Nearby Bike and Pedestrian Facilities, Both Existing and Planned	<ul style="list-style-type: none"> Monitor Divvy bike uses to understand ridership trends. Continue discussing the Leland Avenue Greenway and potential north-south low stress bike routes. Partner with CDOT to strategically install bike racks and other bike facilities, such as repair stations, as appropriate. 	SSA	CDOT

PROGRESS | SUPPORT LOCAL BUSINESSES AND GUIDE DEVELOPMENT: IMPLEMENTATION PLAN

PROJECT	ACTIONS	LEAD	PARTNERS
Objective 1. Preserve Character			
1.1 Support events Programming	<ul style="list-style-type: none"> Continue working with local businesses to coordinate festivals and events in public areas. Continue to create attractive and engaging promotional materials for special events. Partner with businesses in Rockwell Crossing to expand events and fests. 	SSA/LSRCC	Local businesses
1.2 Market Local Businesses With Institutions	<ul style="list-style-type: none"> Continue to promote local businesses on the Lincoln Square Ravenswood Chamber of Commerce website and social media. Encourage greater procurement and patronage between anchor institutions and local businesses. 	SSA/LSRCC	Local businesses / local institutions
1.3 Improve Access and Wayfinding.	<ul style="list-style-type: none"> Develop a wayfinding signage to direct visitors to municipal destinations and cultural institutions. Erect business directory kiosks that advertise local businesses to pedestrians. Establish a blade signage program, through which businesses can design, fabricate and install blade signs in tandem with one another. Assist local businesses with social media and website development. 	SSA/LSRCC	Local businesses / local sign fabricators
Objective 2. Reclaim space for people who bike and people who are walking.			
2.1 Encourage Developers to Lease to Local Businesses	<ul style="list-style-type: none"> Provide financial support for facade and building improvements through Small Business Improvement Fund (SBIF) and the SSA's facade rebate program. Continue to develop marketing campaigns that promote local businesses in Lincoln Square. Coordinate with developers and property owners to create accommodating commercial spaces for small and local businesses. Host events and programs wherein local businesses can network, plan joint marketing efforts, collaborate on common issues (such as loading zones) and make connections that can benefit business practices. 	SSA/LSRCC	Property owners / local businesses
2.2 Coordinate City/County Incentives for Small Businesses	<ul style="list-style-type: none"> Continue to provide outreach and assistance to local businesses. Coordinate with the Aldermanic offices to provide streamlined services to local businesses. Remain on call to assist businesses that need help understanding and accessing beneficial programs. 	CDOT	Aldermen, SSA Transportation

PROJECT	ACTIONS	LEAD	PARTNERS
<p>2.3 Continue and Expand Local Events to Market Local Businesses</p>	<ul style="list-style-type: none"> Conduct pre and post event outreach to understand how businesses are impacted and how to effectively market them. Expand local events to other interested SSA areas, such as Rockwell Crossing. Create marketing campaigns that cross promote local businesses, encouraging visitors to extend their stays. Work with businesses to develop promotions that encourage visitors to shop after festivals. 	SSA/LSRCC	Local businesses
<p>2.4 Create a Pop-up Program</p>	<ul style="list-style-type: none"> Develop a business plan for a pilot pop up business in Lincoln Square, and help them activate a vacant storefront. Determine necessary costs for staffing, insurance and related costs. Recruit 'launch' businesses. Secure vacant space to launch the program, securing at least a three-month commitment from a property owner. Develop a marketing plan for the pop up program. Conduct minor storefront improvements if necessary. Launch a pilot project for the pop up program. If successful, determine the business model. 	SSA/LSRCC	Aldermanic offices / local businesses

Objective 3. Fill Vacancies

<p>3.1 Marketing Materials</p>	<ul style="list-style-type: none"> Expand upon the existing vacancy database to identify available square footage and available amenities. Create and regularly update marketing materials that describe each corridor and available vacancies. Distribute marketing materials throughout the SSA and to other targeted groups that communicate current available opportunities. Coordinate events and marketing efforts for each corridor tailored to the businesses and activities that best fit the area. 	SSA/LSRCC	Aldermanic offices / local businesses / development community
<p>3.2 Renovate and Redevelop Older Spaces</p>	<ul style="list-style-type: none"> Host renovation workshops in which property owners, businesses and local contractors can discuss opportunities for redevelopment. Continue to market the SSA facade rebate program. Consider expanding the facade rebate program to include interior renovations and storefront displays. 	SSA/LSRCC	Local businesses / local contractors
<p>3.3 Encourage Ground Floor Commercial at Key Development Sites</p>	<ul style="list-style-type: none"> Distribute design guidelines to Aldermanic offices. Host meetings with prospective developers to discuss community preferences and design guidelines pertinent to their proposed sites. Introduce prospective developers to local businesses who wish to rent commercial space. 	SSA/LSRCC	Aldermanic offices / development community / property owners
<p>3.4 Support Non-Retail Uses As Appropriate</p>	<ul style="list-style-type: none"> Market first floor spaces as opportunities for services and office uses. Introduce property owners and developers to organizations who wish to rent commercial space. Host meetings with property owners and local businesses to review design guidelines pertaining to first floor storefronts. 	SSA Transportation, Divvy, CDOT	

PROJECT	ACTIONS	LEAD	PARTNERS
Objective 4. Encourage a range of residential			
<p>4.1 Increase Housing Type and Variety</p>	<ul style="list-style-type: none"> • Work with developers and Aldermen to include affordable units in new market-rate, mixed-use developments, and encourage senior housing where appropriate. • Identify older buildings that could be renovated to include affordable rental units. Identify incentives such as Historic Tax Credits and Housing Tax Credits to help subsidize the costs of renovating older buildings. 	SSA/LSRCC/Aldermanic Offices	Property owners / development community
<p>4.2 Encourage Transit-Oriented Development That Includes Residential Density</p>	<ul style="list-style-type: none"> • Promote a diverse range of housing options that include TOD. • Provide local information to new residents so to connect them with community resources. 	SSA/LSRCC/Aldermanic Offices	Property owners / development community
Objective 5. Promote quality development that reflects community vision and corridor character			
<p>5.1 The Square And South Lincoln -Avenue A Walkable, European-Style Street Where Neighbors Gather And Attend Special Events</p>	<ul style="list-style-type: none"> • Work with Aldermanic offices to review design guidelines. • Educate development community and property owners of design guidelines. 	SSA, Aldermanic offices	Property owners / development community
<p>5.2 North Lincoln Avenue A Redeveloping, Mixed-Use Corridor Surrounded By High Quality Neighborhoods</p>			
<p>5.3 : Lawrence Avenue A Developing Mixed-Use Corridor With Opportunities For Greater Pedestrian And Bike Mobility</p>			
<p>5.4 Montrose Ave An Auto-Oriented Corridor With Access To Cta, Community Institutions And High Quality Neighborhoods</p>			
<p>5.5 Montrose Ave An Auto-Oriented Corridor With Access To Cta, Community Institutions And High Quality Neighborhoods</p>			
<p>5.6 Rockwell Crossing A Walkable Commercial Enclave With Access To The CTA</p>			

REINFORCE PLACE | FOSTER PLACEMAKING + A VIBRANT COMMUNITY: IMPLEMENTATION PLAN

PROJECT	ACTIONS	LEAD	PARTNERS
Objective 1. Identify and develop near term projects			
1.1 Murals	<ul style="list-style-type: none"> Continue to work with property owners and local artists to install murals in key corridors. 	SSA PaintWorks	Property owners /host businesses/ artists
1.2 Bike Racks	<ul style="list-style-type: none"> Continue to install bike racks near businesses and streets frequently used by cyclists. 	SSA	CDOT
1.3 Mosaic Planters	<ul style="list-style-type: none"> Work with a local artist to test and understand the materials, costs, and overall feasibility of covering planters with mosaic tiles. Develop a program in which local artists would be assigned planters to cover with mosaic tiles. 	SSA	Local artists / LSRCC Businesses / One River
1.4 Blade Signs	<ul style="list-style-type: none"> Review local sign codes and ordinances Identify a cluster of businesses to test a blade signage program. Secure a local sign fabricator. Secure permits with CDOT to place signs in desired areas. Host a workshop to design signage that evokes specific businesses and meets local codes. Work with the sign fabricator to install signs. 	SSA/LSRCC	Chicago DPD / Property owners / businesses / designer / fabricator / installer
1.5 Parklets	<ul style="list-style-type: none"> Obtain information from CDOT regarding parklet design standards. Identify a business to sponsor a test parklet. Design a parklet with input from the business and community. Secure permits with CDOT to place parklets in desired areas. Install the parklet and activate it with regularly scheduled events. 	SSA/LSRCC	Property owners / businesses / designer / installer
1.6 Develop gateway poles and banners	<ul style="list-style-type: none"> Design a gateway poles based on existing versions along east Lawrence Avenue. Secure permits with CDOT to place new poles in desired areas. Install gateway poles and banners in areas needing better branding, as identified by the community. 	SSA	Greater Rockwell Organization / CDOT / designer / fabricator / installer
1.7 Wayfinding and Business Kiosks	<ul style="list-style-type: none"> Design a business sign kiosk prototype. Secure permits with CDOT to place kiosks in desired areas. Erect business kiosks in areas needing better signage, as identified by the community. 	SSA/LSRCC	CDOT / local businesses / designer / fabricator
1.8 (SQU)Area Plazas	<ul style="list-style-type: none"> Select locations to construct (SQU)Areas that have active, adjacent land uses. Work with CDOT to design and permit (SQU)Areas. Install (SQU)Areas and activate them with regularly scheduled events. 	SSA	CDOT / local artists / designer / fabricator / installer

PROJECT	ACTIONS	LEAD	PARTNERS
Objective 2. Partner with local agencies to develop long range projects.			
2.1 Lincoln Avenue Festival and Market Street	<ul style="list-style-type: none"> Review preliminary concepts with CDOT representatives to discuss logistics for each. Work with a designer to develop project budgets and determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options and testing. Work with CDOT to conduct a traffic test using paint and signage to simulate the selected parking and streetscape pattern. Monitor the test, record data and maintain outreach with businesses and residents to record reactions. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	SSA/LSRCC/Aldermanic offices	CTA/CDOT/Local businesses / property owners
2.2 Giddings Plaza / Alleyway	<ul style="list-style-type: none"> Review preliminary concepts CDOT representatives to discuss logistics and permitting. Work with a designer to develop project budgets and determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape improvement plans. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	SSA/LSRCC/Aldermanic offices	CTA/CDOT/Local businesses / property owners
2.3 CTA Station Plaza / Pedestrian Link beneath the CTA tracks / Market and Events Plaza	<ul style="list-style-type: none"> Review preliminary concepts with CDOT and CTA representatives to discuss logistics and permitting. Work with local artists and designers to install near term projects , such as mosaic planters and murals, to build near term support. Work with a designer to develop project budgets and determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	SSA/LSRCC/Aldermanic offices	CTA/CDOT/Local businesses / property owners / historical associations
2.4 Lincoln Statue Plaza	<ul style="list-style-type: none"> Review preliminary concepts with CDOT and CTA representatives to develop a traffic analysis and study streetscape improvements, including landscaped medians, improved crossings, bump outs and gateway signage. Upon review of the traffic analysis, determine roadway and streetscape requirements. Work with a designer and engineer to develop project plans and budgets, and to determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options. Work with a designer and engineer to develop final design plans and budgets. Prepare project for implementation via RFP or local contracts as budgets dictate. 		

PROJECT	ACTIONS	LEAD	PARTNERS
<p>2.5 Neighborhood Flex Space</p>	<ul style="list-style-type: none"> Review preliminary concepts with Aldermanic offices to discuss project logistics. Work with a designer to develop project budgets and determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape enhancements. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	<p>SSA/LSRCC/Aldermanic offices</p>	<p>Local businesses / property owners</p>
<p>2.6 Lawrence Avenue Complete Street</p>	<ul style="list-style-type: none"> Continue working with CDOT to guide streetscape construction west of Western Avenue. Communicate streetscape plans and relevant building design guidelines with prospective developers, establishing a common vision for the corridor’s long term revitalization. Work with Aldermanic offices to determine opportunities for near term enhancements. Work with a designer to develop project budgets and determine funding opportunities for near term enhancements. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape enhancements. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	<p>SSA/LSRCC/Aldermanic offices</p>	<p>CDOT/Local businesses / property owners</p>
<p>2.7 North Lincoln Avenue Complete Street</p>	<ul style="list-style-type: none"> Review preliminary concept plans developed within the Master Plan report with CDOT and CTA representatives to discuss logistics towards developing a traffic analysis and determining feasibility for streetscape improvements, including bus bulbs, medians and widened sidewalks. Upon review of the traffic analysis, determine roadway and streetscape requirements. Work with a designer and engineer to develop project plans and budgets, and to determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	<p>SSA/LSRCC/Aldermanic offices</p>	<p>CTA/CDOT/Local businesses / property owners</p>

PROJECT	ACTIONS	LEAD	PARTNERS
<p>2.8 Western Avenue Boulevard-ization</p>	<ul style="list-style-type: none"> Review preliminary concepts with CDOT and CTA representatives to develop a traffic analysis and study streetscape improvements, including landscaped medians, improved crossings, bump outs and gateway signage. Upon review of the traffic analysis, determine roadway and streetscape requirements. Work with a designer and engineer to develop project plans and budgets, and to determine funding opportunities. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options. Work with a designer and engineer to develop final design plans and budgets. Request proposals for construction of the final concept. 	<p>SSA/LSRCC/Aldermanic offices</p>	<p>CTA/CDOT/Local businesses / property owners</p>
<p>2.9 : Rockwell Crossing Shared Street</p>	<ul style="list-style-type: none"> Work with a designer and engineer to develop preliminary concept plans for Rockwell Crossing. Work with local artists and designers to install near term projects, such as parklets and seating areas, to build local support. Review preliminary concepts with CDOT and CTA representatives to discuss logistics and permitting for streetscape improvements. Conduct a workshop style meeting with Aldermanic offices, local businesses and residents to discuss streetscape options. Work with a designer and engineer to develop project budgets and determine funding opportunities. Request proposals for construction of the final concept. 	<p>SSA/LSRCC/Aldermanic offices</p>	<p>CTA/CDOT/Local businesses / property owners</p>

END OF MAIN DOCUMENT

